

B. Community Form

Goal: Reinforce the Nob Hill Highland district as a vibrant, high quality, pedestrian-oriented urban community and a destination for greater Albuquerque.



1. The Vision

The community envisions Nob Hill/ Highland (NHH) to continue as an urban village alive with people and activity. Residents will walk easily and comfortably from their homes to neighborhood stores, restaurants, coffee shops services bordering Lomas and Central Avenue. NHH will be the place where people from all over the metropolitan area go to experience true urban street life with its critical mass of sidewalk cafés, theaters, art galleries, coffee shops, and thriving small businesses. An eclectic mix of local, regional and national businesses will serve local as well as regional markets.

There will be more people living in NHH than there are today, creating greater residential density close to the Central corridor and greater diversity in housing, income levels and types of residents. Seniors, college students, empty-nesters, wealthy

or low-income people can choose to live in a variety of upper story lofts, courtyard and terraced apartments, live/work units, stacked lofts over flex space, patio homes, flats, rowhouses, carriage houses, townhouses, and studios. Space design will be flexible, often combining residences with new live/ work commercial space. Residents will have grocery stores, outdoor markets, bakeries and other necessities of daily life available within walking distance.



Existing historic residential districts will be preserved and new districts registered. These historic neighborhoods of small bungalow homes will continue to be valued and preserved. Additions to existing homes and infill development will support the unique character and qualities of these neighborhoods. They will be well maintained with open, high-quality streetscapes so that everyone can enjoy their unique architectural variety. These streetscapes, several configured as boulevards, will include pedestrian lighting utilizing state-of-the-art fixtures. Residential neighborhoods will have on-street parking in front of their homes. Neighborhood transitional

areas will be protected from the impacts of noise, traffic and parking generated by commercial activity. Overhead utilities will be relocated underground.



NHH will continue to have character and charm, and this character will extend further east along the commercial corridor with gradually increasing heights and densities toward San Mateo and the Highland area. Architectural styles will be eclectic and unique to the place. The historic scale and character of the original commercial district will be retained. Pedestrians will continue to experience great views of the sky and mountains. Buildings will have high quality design and interesting style. Public gathering spaces will draw the community together for celebrations, musical events, art shows, and open markets. NHH will not imitate other places; the district will continue to pioneer its own unique



definition of southwestern urbanism. Nowhere else in the city or country will look like Nob Hill; it will continue to have its own valued identity.

Visitors from all parts of the city, country and world will continue to be drawn to Central Avenue (Old Route 66) to experience its unique history and pioneering role in American car culture, enjoying small boutique motels, interesting roadside architecture and signs reflecting different eras. It will still be an authentic tourism landscape. Businesses will display attractive signs (neon, glass or painted) that reflect the style and scale characteristic of historic Route 66.

In the Nob Hill/Highland area, the pedestrian rules. There will not be big parking structures or buildings with blank walls to the neighborhood. There will not be large parking lots bordering the street. Parking will be dispersed in courts that are hidden from view, wrapped with housing or shops, or enclosed in small courtyards. On-street parking will continue to serve small businesses. Many people will be able to live without cars in townhouses, lofts and condos within one-quarter to one-half mile walking distance of the new modern streetcar stations up and down Central Avenue.

Alleys will be enhanced with landscaping and well-maintained by residents from nearby blocks. Alleys will be safe and used as walkways. Some alleys will have carriage houses above garages or other innovative housing that utilizes and activates

formerly dead alley space. Developers building in NHH will be community-minded, innovative, sensitive to historic context, and comfortable with a collaborative approach to design. Design will result from a broad-based public process, and the community will hold development to high standards of innovation, creativity and sensitivity to context. Individual developments will fit with the over-all character of good urban design that is pedestrian scale and contributes to the vitality of the district. Change and new investment will be welcomed and encouraged, but development will be kept at an even pace, and not become too overheated.

Policies

1. The City shall support an eclectic mix of local, regional and national businesses that will serve local as well as regional markets.
2. The City shall support greater diversity in housing, income levels and types of residents.
3. The City shall support existing historic residential districts and the registration of new districts.
4. The City shall support well maintained and high quality streetscapes.
5. The City shall support the creation of public gathering spaces.
6. The City shall support the display of attractive signs that reflect the style and scale characteristic of historic Route 66.
7. The City shall support the development of parking that will be dispersed in courts and structures that are hidden from view.
8. The City shall support well-maintained alleys that are enhanced with landscaping.

2. Commercial Districts

Recognizing that the Nob Hill Highland Sector Plan area is comprised of distinct sub-areas with unique qualities, the following sections of the report divide the commercial and residential areas into "districts." Issues of *Use, Height and Scale, Signage, Transition and Historic Preservation* are addressed within each specific district as a way of guiding future development. The result will be districts that possess distinct and identifiable character but that also aesthetically relate to adjacent districts in ways that contribute to a shared identity throughout the entire Sector Development Plan area.

There are three commercial districts that cover the length of the Central Avenue "corridor" from Girard to San Mateo. In this context, the "corridor" refers to Central Avenue as well as commercial areas on Silver and Copper Avenues and Lomas and San Mateo Blvds. The plan further addresses issues of commercial activity and on establishing pleasant transition zones between commercial areas and adjacent residential communities. The three commercial districts are:

Route 66 Historic Core: from Girard to Carlisle
Emerging Nob Hill: from Carlisle to Washington
Highland: from Washington to San Mateo and San Mateo to Lomas

The Central Avenue corridor within the plan area is expected to see significant commercial and residential growth in coming years. The Nob Hill Highland Sector Development Plan seeks to create regulations and guidelines that stimulate commercial growth and intensification in a manner that respects and enhances the quality of life in neighboring residential areas. Attention is therefore given to issues such as transition zones between



residential and commercial areas (i.e. appropriate scale of buildings, entrances to retail from both back and front sides, and the reduction of large paved parking lots in front of businesses), pedestrian-friendly design (i.e. outdoor seating areas, ample shade, and storefronts that interact with sidewalk traffic through storefront windows and outdoor displays), and a vibrant mixing of residential and commercial uses within the corridor.

An important component of any commercial corridor is its scale as related to the pedestrian. Some commercial streets “feel” more comfortable to the pedestrian because the building heights and the “street wall” it presents create a sense of enclosure that feels more like an outdoor room than a linear strip development along a major arterial. Many studies have been conducted to try to determine the appropriate proportions of building height to street width that creates this optimum sense of place along a commercial corridor. The Main Street Center of the National Trust for Historic Places has carried out extensive research on this issue and have found that the ideal proportions for commercial streets is between 1:2 and 1:3. That means that the building height is one third to one half the width of the street right of way. This is illustrated in the following diagrams, Figure 24 From the Main Street Center’s “Main Street Handbook”. The diagram also illustrates a scale that is “uncomfortable for pedestrians” at a ratio of 1:7. A section through a typical location on Central Avenue shown in Figure 25 shows that the building height to street width ratio is 1:6.4. This would suggest that for the Central Avenue corridor and its commercial districts to become a more comfortable place for pedestrians that one of the issues that needs to be addressed is the allowable height of buildings along the corridor. To achieve a ratio in the 1:3 to 1:2 range building heights

would need to range from three to five stories along Central Avenue. Addressing building heights is part of the strategy for improving Nob Hill Highland’s commercial districts.

The following policies, developed from community input, apply to all three commercial districts within the Nob Hill Highland Central Avenue Corridor.

Policies

1. The City shall increase densities in the designated Metropolitan Redevelopment Area.
2. The City shall encourage adaptive reuse of old gas stations and motor courts.
3. The City shall support the attraction of positive activity to the area day and night.
4. The City shall support the increase of housing choice and density along Central Avenue.
5. The City shall support the integration of different types of housing (live/work spaces, stacked flats, etc.).
6. The City shall support the retention and encouragement of retail and services that meet the needs of local residents.
7. The City shall establish a green space/ park/multiple use site on Central Avenue.
8. The City shall support the preservation of historic landmarks and streetscapes.
9. The City shall emphasize pedestrian-oriented design in the area.
10. The City shall help create smooth transitions to and access from adjacent neighborhoods.
11. The City shall model the past successes of the Nob Hill Route 66 Historic Core renaissance.
12. The City shall support the creation of outdoor rooms: open patios, landscaped courtyards, plazas, oases.
13. The City shall support the creation an interesting and varied street wall along Central Avenue.

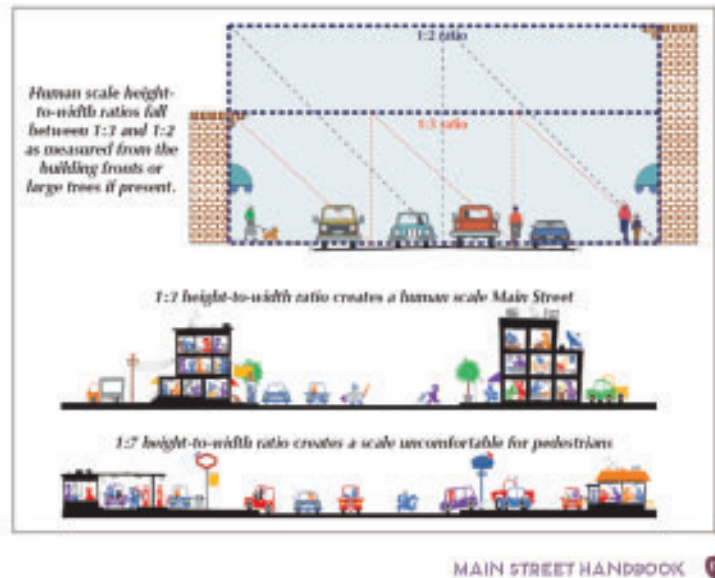


Figure 24: Illustration of Desirable Height-to-Width Ratios



Typical existing condition

Figure 25: Typical Cross Section through Central Avenue Corridor

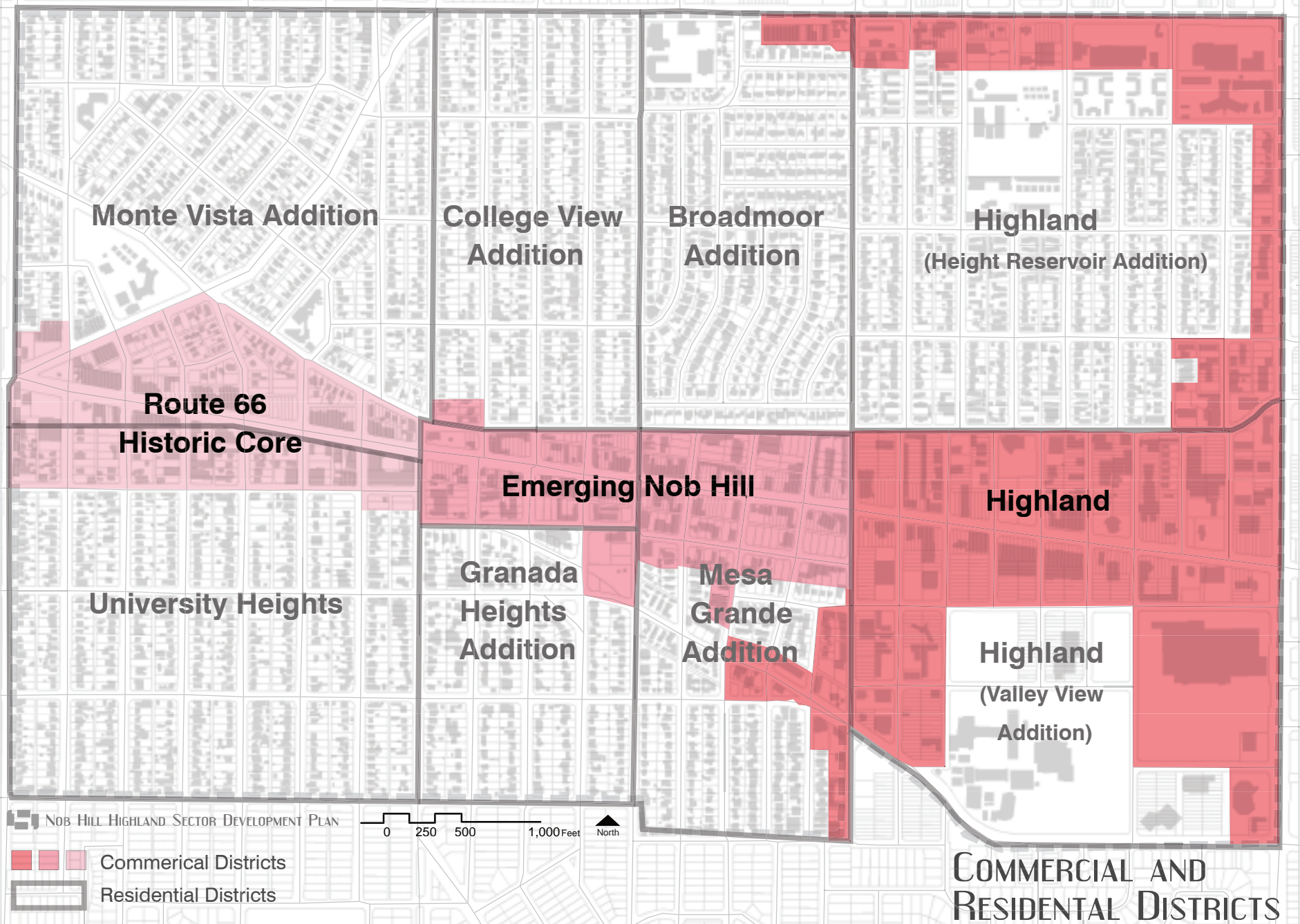


Figure 26: Commercial and Residential Districts



District 1: Route 66 Historic Core (Girard to Carlisle)



Goal: Future development for this district should emphasize designs and uses that fit within the existing context such as architectural style, outdoor eating spaces, pedestrian orientation and a relaxed atmosphere and provide expanded retail and upper story urban residential living opportunities.

The Route 66 Historic Core District developed primarily in the 1920s and 1930s and is emblematic of an eclectic roadside architecture made famous by travelers of the “Mother Road.” Styles such as Streamline Moderne, Pueblo Revival and Spanish Territorial Revival are all well represented, with several buildings on the National Register of Historic Places and many others considered as historically significant. Notable features include building designs that step down in response to topography and slope, classic small-scale lot sizes of 25’ X 150’, careful attention paid to transitions between commercial and residential areas, and a use of design features such as towers and vibrant colors. The Nob Hill Shopping Center at Carlisle and Central was built in 1937 and has come to represent key features that local residents find desirable and which should be incorporated into future area development. These include

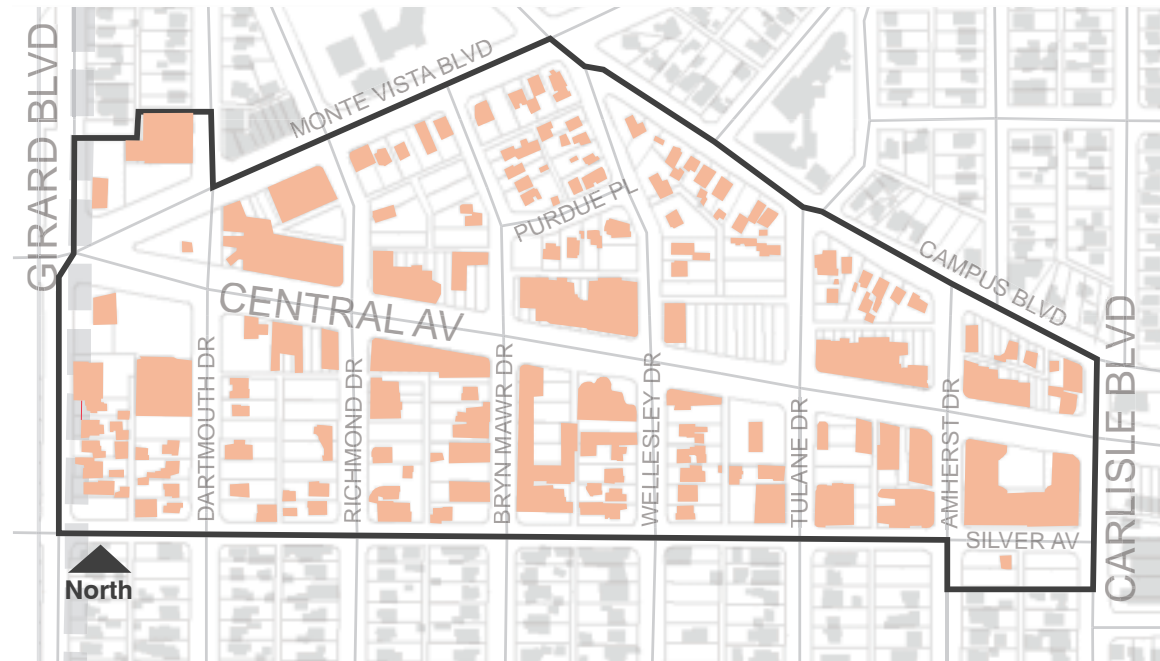


Figure 27: Historic Core District Map

storefronts that face both Central Avenue and residential areas to the north, breezeways that encourage easy access to the interior court and a low profile façade that is scaled for pedestrian-use.

Policies

1. The City shall preserve contributing historic buildings and existing scale in the district.
2. The City shall support the design of commercial/residential transition areas to provide easy access from neighborhoods and protect the integrity of historic districts.

Actions

1. **Use** -The Route 66 Historic Core District has a tradition of providing an eclectic

collection of goods and service options to the traveler as well as the resident. Maintaining this variety is critical to the continuing success of this unique area as a destination for the neighborhoods, the Albuquerque metropolitan area as well as visitors to the region. While the street level should be charged with exciting retail and entertainment options, the upper levels of buildings adjacent to Central Avenue should provide opportunities for office activities and a wide array of housing choices. Bringing housing to the corridor will further enrich this area, provide 24 hour “eyes on the street” as well as support this emerging major transit corridor.

2. **Height and Scale** - Buildings should vary in height but be sensitive to the transition to the adjacent building context particularly relative to historic buildings. Façades should maximize the amount of clear single pane glazing and avoid blank walls to the street. Attention should be paid to preserving the historic wainscoting at the base of the façade and reflecting traditional materials such as tile in those areas. Buildings in this district shall be either one, two, or three stories along Central Avenue and either one or two stories where they face single-family dwellings in adjacent residential neighborhoods. Where possible the location and height of buildings should frame views to the mountains.

Maintaining the diversity of building form and design is particularly critical to reinforcing the traditional character and scale of this district. All buildings should front on the street, and façades should not be flat but should vary in their relationship to the property line. Building design should respond to the historic 25' lot module that significantly influences the scale and character of this historic district. Buildings should celebrate Route 66 and include interesting towers where appropriate. They should create an attractive public face on all sides of buildings facing streets or alleys.

3. **Signage** - A major challenge in the Nob Hill Historic Core is to encourage sensitive signage which reflects the history and character of the district, and to discourage run-of-the-mill signage which contributes to visual clutter and transforms building façades into billboards. Historically, two types of signs were dominant in the Nob Hill district: the wall-mounted, individually-lettered signs popular during the 1930s, and the vertical projecting signs which

grew in abundance during the 1950s. Both made prodigious use of neon and exhibited a style which varied from the high style of 1930s art deco to the complexity of the neon jungle. Historic neon signs should be protected and restored wherever possible and new signs should take full advantage of the many exciting examples of this rich heritage of neon art.

4. **Transitions** - In this district it is critical to define appropriate scale, massing and design in order to ensure that new development blends in to protect single-family residential amenities for the people who live there. Build smooth seamless, pedestrian-friendly and aesthetically pleasing transitions from commercial to residential areas. Avoid harsh transitions. Commercial buildings should not dominate residential areas and commercial parking should not congest neighborhoods. Treat side street and back façades with the same care given to Central Avenue façades. Wrap developments so that each street front has active uses and well designed façades. Create "outdoor rooms", i.e., open patios, landscaped courtyards, plazas, and oases. Use appropriate transitional scale that steps down in height and with topography, preserves small structure and incorporates medium density residential housing, such as townhouses, stacked flats, rowhouses and small businesses, which relate both to the neighborhood and the commercial environment.

5. **Historic Preservation** - Celebrate old Route 66. Encourage innovative eclectic architectural styles, allow flexibility along with elements of Route 66 roadside

architecture and the original styles of historic register landmarks to retain the essence of Nob Hill character. Create a sign landscape that displays renovated historic signs and signs that are consistent in scale and design with Route 66 character including elements such as neon, glass and painted motifs. Respect and preserve the special character of the Historic Core as a valued resource through preservation or adaptive reuse of existing historic buildings including the Campell's Grill, Hendren Building, Monte Vista Elementary School, Lobo Theater, Monte Vista Fire Station, Jones Motor Company, Nob Shopping Center and others as shown on the District Map, Figure 27.



Implementation

The Plan Implementation section of this document, Section 5, presents a consolidated list of the above recommendations and delineates the City agencies and potential funding sources vital to their realization. Recommendations fall into one of the following categories: Capital Improvements Projects, Program Enhancements, or Community Initiatives, as elaborated in Section 5.A.; or they are established through zoning regulations, as elaborated in Section 5.B. Form-based zoning regulations include design requirements to create a land-use regulation environment focused on achieving the community form goals described above.

District 2: Emerging Nob Hill (Carlisle to Washington)



Goal: Future development should take advantage of the recently established Metropolitan Redevelopment Area designation to establish an upscale entertainment district with art galleries, antique shopping, live/work spaces and neighborhood services that continue the core Nob Hill character.

The Emerging Nob Hill District was developed after the Route 66 Historic Core District, mostly between the 1940s and 1960s. This commercial area was primarily designed for access by automobile and as a consequence lacks many of the pedestrian amenities valued by local residents. Much of the area is dominated by old motor courts that constitute a defining feature of Route 66 history. Notable neon signage and a proliferation of antique shops have begun to forge an identity for the district which is in many ways an extension of development patterns established within the Route 66 Historic Core.

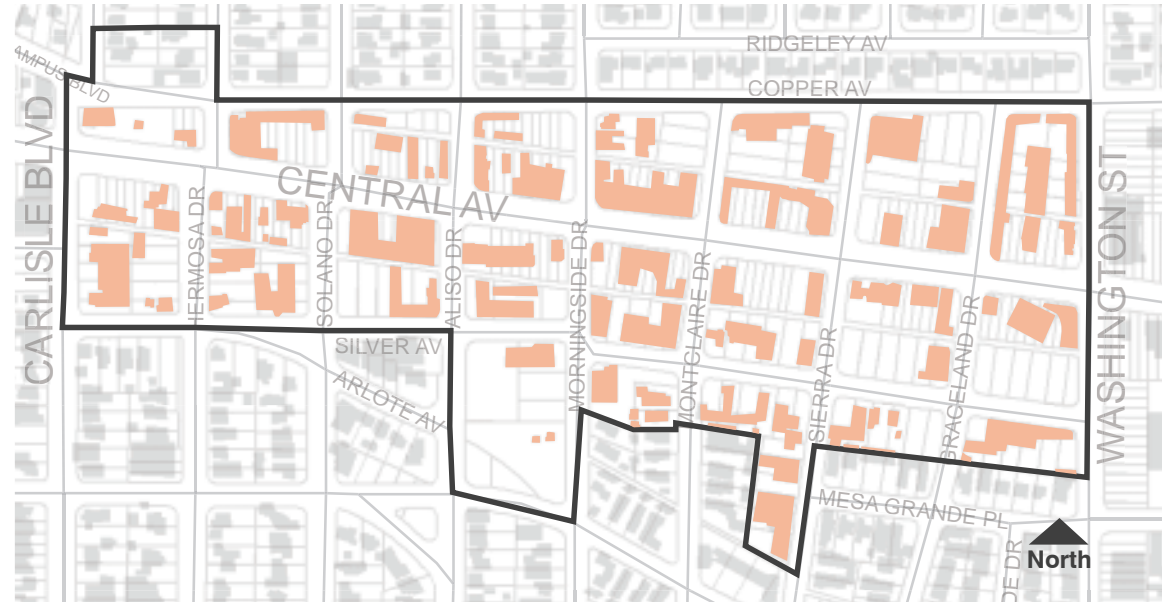


Figure 28: Emerging Nob Hill District Map

Policies

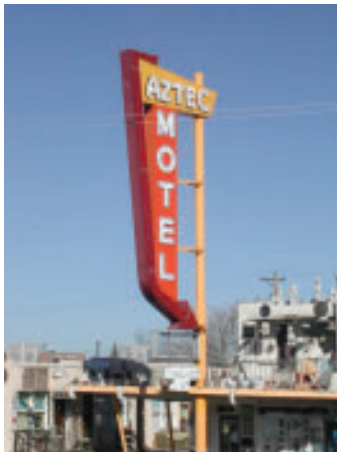
1. The City shall utilize an urban template of mixed scale, mixed use (residential/commercial) and mixed local/national retail to address underutilized or blighted properties.
2. The City shall encourage building designs that are consistent with Route 66 history and neighboring Nob Hill.
3. The City shall use creative design and infill development to ease transition from commercial to residential areas.

Actions

1. **Use** - The Emerging Nob Hill District should be redeveloped as a transition between the historic uses and character of Nob Hill coupled with enhanced

opportunities for upper story urban living, adaptive reuse of the historic motor courts and expanded entertainment options. It should develop pedestrian-oriented live/work spaces that are built for versatile uses. The district should provide upscale entertainment ranging from galleries to dining to theatre that will attract positive activity day and night. It should also encourage retail and diverse services such as theme restaurants, home furnishings and apparel stores to meet the needs of local residents. East of Morningside larger retail establishments that are designed to fit into the district context may provide walkable access to needs such as hardware and groceries. These types of buildings should be "wrapped" with smaller shops where appropriate.

2. **Height and Scale** – Reorient building entrances to Central Avenue. Manage scale to avoid “fortress” scale development that dwarfs pedestrians. Retail and residential mixed use development should front on the property line with subtle variation in façade setback. These building shall be either one, two, three, or four stories facing Central Avenue and either one or two stories where they face single-family dwellings in adjacent residential neighborhoods. Nob Hill “Historic Core” design elements should be incorporated into the design of new projects in this area to continue the character of the Route 66 corridor.
3. **Signage** - The signage in this area should reflect the traditions of historic Route 66 while at the same time allow for more contemporary design themes and materials. Signage should be consistent with the City of Albuquerque Sign Ordinance requirements but should prohibit backlit box signs.



4. **Transitions** - Create seamless transitions between commercial areas and neighborhoods; add housing to commercial areas, especially medium density townhouses, stacked flats, etc. facing the neighborhood. New development should establish a comfortable and appealing pedestrian-scale transition from Central Avenue both north and south to the adjacent neighborhoods. Treat side street and back façades with the same care given to Central Avenue façades. Wrap developments so that each street front has active uses and well designed façades. Provide wide sidewalks and a continuity of landscape that ties transition streets to a beautifully designed Central Avenue.
5. **Historic Preservation** - Foster creative adaptive reuse of Route 66 historic motor courts for creative tourist-oriented uses (e.g. spas, shops, renovated boutique motels). Respect and preserve the special character of the Route 66 corridor through preservation or adaptive reuse of existing historic buildings including the Immanuel Presbyterian Church, Nob Hill Motel, Aztec Motel, De Anza Motor Lodge, People’s Flower Shop, Toddle House (Jamaica-Jamaica), Nob Hill Shoe Repair Building, Morningside Antiques and others as shown on the District Map, Figure 28.

Implementation

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District 3: Highland Commercial District (Washington to San Mateo)



Goal: Intensify the area's density with a pedestrian-oriented mix of commercial and residential uses that enhance the antique retail, performing arts and theater entertainment qualities that currently exist.

The Highland Commercial District has developed primarily since the 1950s. The area contains many large scale commercial properties (primarily offices) as well as an extension of antique stores and entertainment uses found in the Emerging Nob Hill District. This district is the least pedestrian-friendly area within the Plan area's Central Avenue corridor with a poorly-developed street wall and a proliferation of large, paved surface lots between buildings and the street edge. As part of the Metropolitan Redevelopment Area, the district is proposed for significant new redevelopment activity.

Policies

1. The City shall create a high density, pedestrian friendly, urban identity for the Highland Commercial Areas.
2. The City shall ensure future development integrates with surrounding neighborhoods.

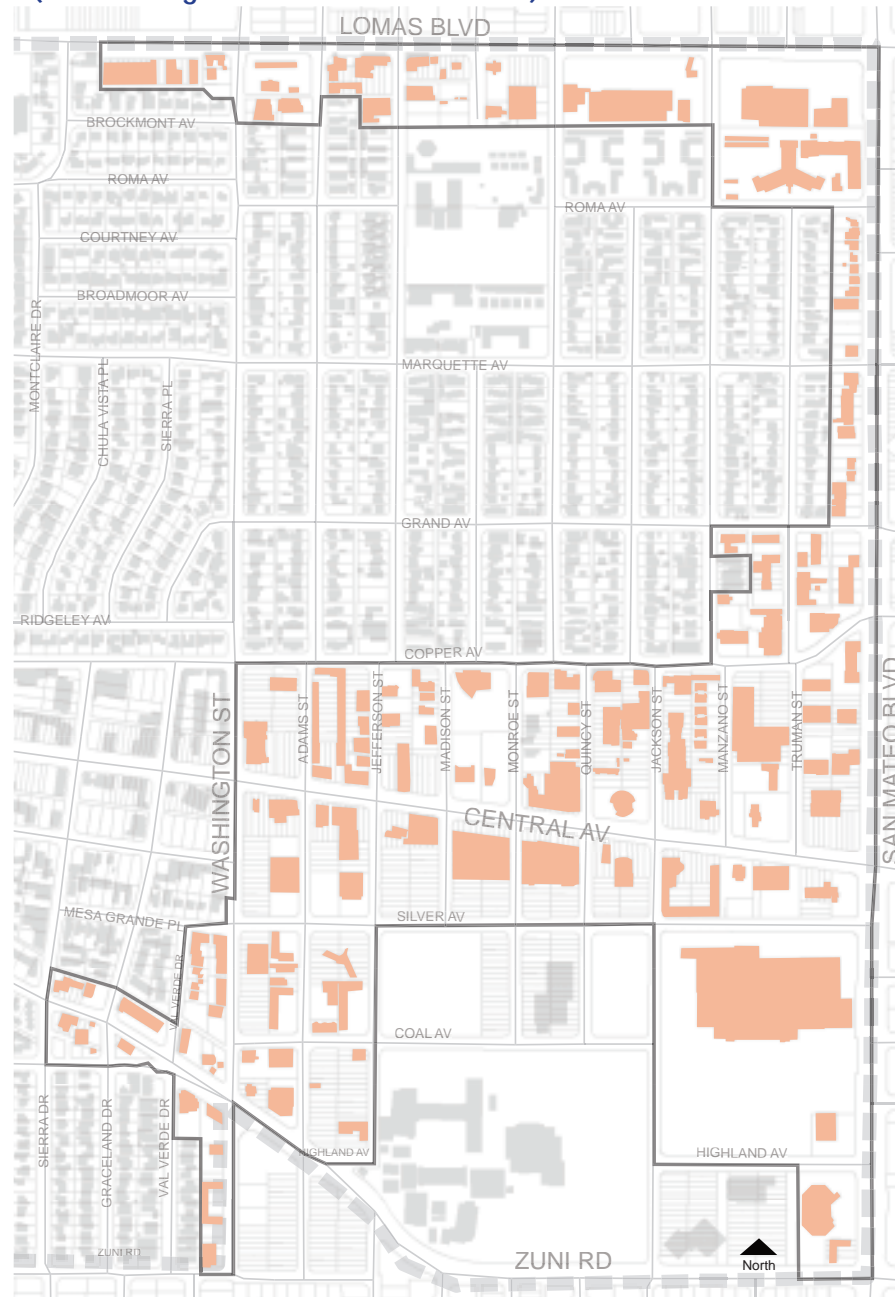


Figure 29: Highland Commercial District Map

Actions

1. **Use** - A grouping of major theme restaurants located near Central and Sierra could anchor the antiques district, help to pull pedestrians from Nob Hill and reinforce existing restaurants and clubs in the area. Neighborhood retail businesses appropriate in this area could include services which residents utilize on a more frequent basis such as dry cleaners, small food stores (farmer's markets, bakeries, etc.), coffee shops, gas stations, video rental stores, etc. Regarding community retail, the recently expanded Super Wal-Mart is one of the area's largest draws and could help to support numerous additional community level retailers. These stores could include hardware stores, renovation-oriented stores, sporting goods, office supply, apparel discount department stores, book sellers, home furnishing, electronics, etc. Office and a wide variety of housing choices should populate the upper stories of all of the buildings developed in the area to support the retail, entertainment and transit focus of this district.
2. **Height and Scale** - This district should promote pedestrian-oriented redevelopment of mixed use projects at higher densities, heights and scale. Infill with "junior box" retail, lofts and offices should offer pedestrian orientation at the street level to create vibrant street life day and night. The area around the Hiland Theater, which has been identified as a major transit stop, has the potential to be a transit oriented development area. New development should provide ground level retail, commercial service and entertainment opportunities as well as

upper level medium to high density living units within walking distance to bus and fixed rail transit options. Building heights in this district shall be either one or two stories on the northern edge where they face single family dwellings and either one, two, three, four, or five stories along Central Avenue and to the south. There should be appropriate step backs along the streets bordering adjacent residential areas as buildings transition to their maximum height.

3. **Signage** - The signage in this area should reflect the traditions of historic Route 66 while at the same time allow for more contemporary design themes and materials. Signage should be consistent with the City of Albuquerque Sign Ordinance requirements but should prohibit backlit box signs.
4. **Transitions** - Optimize building height and massing transition on the northern edge of the district adjacent to the neighborhood. The Plan proposes a Lomas Corridor Plan to study and establish zoning, land-use, building height, and streetscape improvements, particularly as they require attention on all four corners of the intersection with San Mateo, which was outside the scope of this Plan. Buildings built adjacent to Copper Avenue should be one or two stories at the property line and should step back appropriately to maintain solar access to properties on the north side of the street. Treat side street and back façades with the same care given to Central Avenue façades. Wrap developments so that each front has active uses and well-designed façades. Maintain pedestrian focus with wide sidewalks and landscape public "resting" areas.



5. **Historic Preservation** - Promote the preservation and adaptive reuse of existing historic buildings including the Classic Century Square, Hiland Theater and Performing Arts Complex, JC Penny Building, Desert Sands Motel sign, Bridgers & Paxton Solar Building and the Old Albuquerque National Bank Building. Utilize the Hiland Theater complex as an arts and entertainment anchor in a new redeveloped transit oriented district on Central Avenue.

Implementation

The Plan Implementation section of this document, Section 5, presents a consolidated list of the above recommendations and delineates the City agencies and potential funding sources vital to their realization. Recommendations fall into one of the following categories: Capital Improvements Projects, Program Enhancements, or Community Initiatives, as elaborated in Section 5.A.; or they are established through zoning regulations, as elaborated in Section 5.B. Form-based zoning regulations include design requirements to create a land-use regulation environment focused on achieving the community form goals described above.

3. Residential Districts

Goal: The community's housing goal is to provide a range of housing opportunities that preserve the established character of the neighborhoods within the plan area while allowing homeowners the freedom to adapt older homes to modern needs.

There are four distinct residential areas in the Plan area, each of which contains a number of subdivisions with distinct qualities and character. The four residential districts reflect the characteristics of different time periods of development as the City grew eastward from the University of New Mexico.

- **Monte Vista/College View Historic District:** roughly bounded by Girard and Lomas Boulevards, Morningside Drive, Copper Avenue and Campus and Monte Vista Boulevards.
- **Broadmoor Addition:** bounded by Morningside, Copper, Washington and Lomas.
- **University Heights, Granada Heights and Mesa Grande Addition:** bounded by Girard, Garfield/Zuni, Silver and Morningside.
- **Highland Neighborhood (Heights Reservoir Addition):** bounded by Washington, Copper, San Mateo and Lomas.

While the neighborhoods in the plan area represent a mix of housing types and styles, residents have common interests that can be advanced through City policy.

Policies

Plan Area

1. The City shall promote housing affordability in the Nob Hill Highland area.
2. The City shall provide for a mix of housing types and prices, maintaining the scale of existing neighborhoods.
3. The City shall increase housing opportunities through mixed use development along commercial corridors.
4. The City shall encourage building additions and renovations that blend with the style of the original building.
5. The City shall encourage residential infill that blends in style and massing with surrounding structures and the streetscape of the block in which it is located.
6. The City shall support the efforts of Sector Plan area residents to pursue historic district designations for areas and individual buildings with registration potential.
7. The City shall support the efforts of Sector Plan area residents to protect solar access.



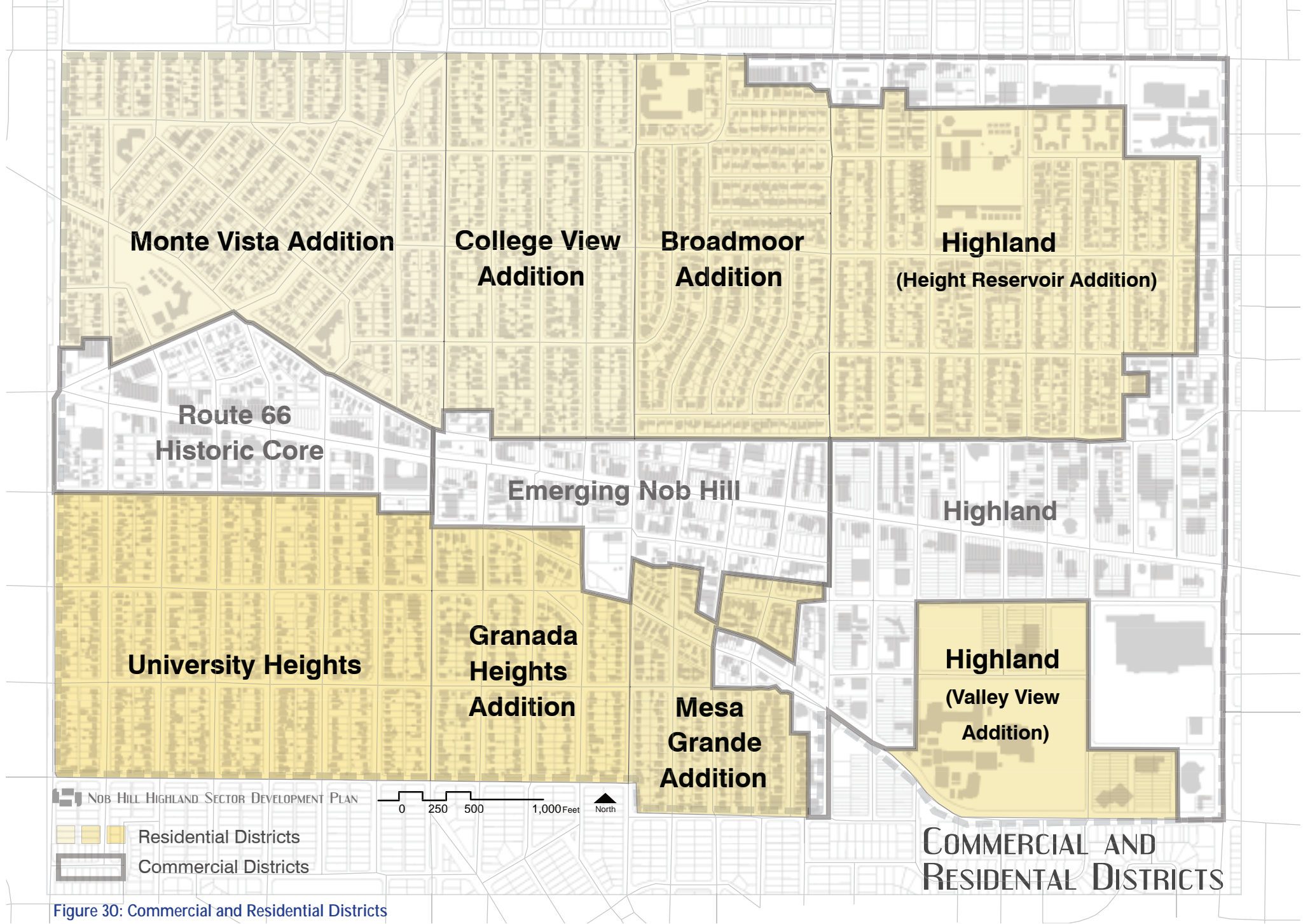


Figure 30: Commercial and Residential Districts



Monte Vista/ College View Historic District



Over 90% of the neighborhoods north of Central Avenue between Girard and Morningside Dr. were built out during the period from 1926 to 1942. Key aspects of the historic appearance of the neighborhood were documented in an inventory of historic buildings completed in 1995. As a result, the historic or architectural value of these neighborhoods has been well-documented. Features that contribute to neighborhood character include one story construction, uniform front yard setbacks with variation by block from 20 feet up to 40 feet, five-foot side yard setbacks, and several predominant architectural styles with characteristic architectural details and materials. Recommendations for this area are intended to preserve the historic character of the area by educating homeowners about the district, features that make it special, and best practices for maintaining and renovating a historic home. Residents want the neighborhood to remain a place that diverse people call home.



Policies

1. The City shall support the efforts of historic district residents to protect the character of the historic district, including residential scale and streetscapes.
2. The City shall protect the integrity of registered cultural properties within the district.



Actions

1. **Height and Scale** - Homes in the district are typically one story, although second story additions have been added to some homes. To maintain the district's historic character, building additions should blend with the original style of the house, maintain the streetscape of the block and be in scale with houses on either side. Zoning within the historic district should be revised to protect the neighborhood's historic scale.
2. **Solar Access** - Lots in the historic district are typically oriented east/west and are relatively narrow – 50 feet in most cases. These properties are excluded from the solar access provisions of the City Zoning Code because of their age. Residents have identified loss of solar access as a consequence of second story additions to homes in the district. The City has a Solar Permit Ordinance, adopted in 1981, which allows property owners to secure solar rights through a permit process if they have a solar collector.

3. **Streetscape** - Walls, carports and other structures in the front yard setback are not part of the historic character of the neighborhood. Walls over three feet in height, carports in the front yard and variances allowing buildings to encroach into the front yard setback were identified by the neighborhood committee analyzing residential character as alterations that significantly impair the district's historic streetscapes. Conditional use requests for walls in the front yard setback over three feet in height and setback variances to allow carports or building additions should not be approved.

4. **Historic Preservation and Neighborhood Conservation** – Homes in the Monte Vista and College View Historic District represent a variety of styles common to Albuquerque in the period from 1926 until the beginning of World War II. Predominant architectural styles include Mediterranean Revival, Southwest Vernacular, Spanish Pueblo Revival, Territorial Revival, Medieval Cottage, and California Ranch. Each of these styles has characteristic details and materials. As the needs of families have changed over time, many of the homes have been remodeled or expanded. Because change will continue, it is important to

make sure that new owners are aware of the historic district and the best ways to improve their homes to retain the character of the house, and ultimately of the district.

An excellent example of an approach that relies on education is the University Neighborhoods History Handbook. Published in 1986 by the University Heights and Silver Hills neighborhoods, with assistance of local businesses, this document contains information about neighborhood history, architectural styles, landscaping, maintenance and renovation, energy conservation and solar retrofits, infill, and commercial buildings.

Implementation

The Plan Implementation section, Section 5, presents zoning code recommendations and projects to accomplish the policies for the Monte Vista and College View Historic District.



Historic College View Homes



University Heights and Granada Heights

The area south of Central between Girard and Washington has areas with similar character to the Monte Vista and College View Historic District, with a mix of single family homes and small multifamily buildings. A variety of architectural styles common to the University area neighborhoods predominate. Basic research and a limited survey of this area were conducted in 1995, and researchers documented potential for part of the area to be designated as a historic district.

Policies

1. The City shall support the efforts of the residents of University Heights to protect historic character.

Actions

1. **Historic Preservation** - University Heights south of Central from Girard to Carlisle and Central to Garfield, and parts of Granada Heights have been identified as having potential for National Register nomination. A survey of structures in this area should be completed, along with district nomination of areas that meet nomination criteria and nomination of individual buildings with registration potential. Other blocks that exhibit similar character to those identified in the 1995 Historic Building Inventory should be reviewed for potential for inclusion in the survey and historic district.

The University Neighborhoods History Handbook is applicable to the Nob Hill Southeast Heights area, and the neighborhood associations in the area should promote this document to educate owners about the neighborhood's history, architectural styles and maintenance and renovation best practices. This document can be reproduced in its current form, or updated as a community initiative.

2. Preservation of Historic Character

A new document specifically tailored to Nob Hill with a focus on renovation of historic homes is recommended. This history and preservation handbook would update and expand the University Neighborhoods History Handbook. Items that could be included are:

- A description with illustrations of historic architectural styles
- Discussion of elements that contribute to historic character
- Recommended approaches to renovation and remodeling.
 - Materials
 - Colors
 - Windows and doors
 - Porches
 - Ornamentation
 - Scale and massing
 - Streetscape
 - Design to blend with original style

An updated handbook should be supported by the City with funding for development and printing of the document.

Implementation

The Plan Implementation section, Section 5, includes the recommendation that the community and the City pursue a survey of homes in University Heights and Granada Heights for potential historic district designation and National or State Register nomination of the district and appropriate individual properties. The plan also recommends that the community promote the University Neighborhoods History Handbook to homeowners in this area.

To further historic preservation goals, a new or updated handbook explaining historic architectural styles and historic character with detailed guidelines for renovation and additions that blend with a home's original architectural style is recommended. As with the original handbook, this could apply to all Central Albuquerque and University area neighborhoods.

Highland Neighborhood



The Highland Neighborhood was built after World War II and has a mix of single-family and small-scale multifamily structures as well as the Plan area's larger apartment buildings. Planning for the City in the 1950s identified the neighborhood as part of a larger "urban center" located at Central and San Mateo. The R-3 zoning that was put in place when the City adopted zoning in 1959 anticipated high density multifamily housing throughout the area, which was inconsistent with established uses at the



time and with the evolution of the neighborhood over the past 50 years. Policies and actions for this area are focused on bringing zoning and land use into better alignment to ensure that public policy protects the quality and character of the neighborhood.

Policies

1. The City shall support zoning consistent with long-established residential development patterns.



Actions

1. **Zoning** - Residential zoning in the Highland area should be reviewed and single-family homes and small-scale multifamily properties should be rezoned from R-3 to a zone that respects the use and scale of neighborhood residences.

Implementation

The Plan Implementation section, Section 5, contains residential zoning recommendations for the Highland neighborhood.



Existing architectural styles



Single family to multi-family conversion dominates lots



Streetscape showing setbacks



Poor transition from commercial to residential.
Commercial building dominates neighbors

4. Public Facilities/Community Services

Goal: Improve the community's access to public facilities and community services.

Public facilities and community services in the Nob Hill Highland area are important to the fabric and identity of the community. As infill occurs and the plan area urbanizes these public facilities and community services will become increasingly important. As stated in section III, the Plan area has a number of facilities with an array of activities offered by each. The community is also served by APD's Triangle Community Substation, Fire Station # 3, and the Highland Senior Center.

Policies

1. The City shall help to foster better communication and cooperation between institutions and the neighborhood to make public facilities as widely available as possible.
2. The City shall work to expand existing community services to better serve the community.
3. The City shall continually evaluate the level of community service in the Plan area to ensure a high level of service.

Actions

The plan encourages establishing improved relationships with government agencies and educational institutions to promote shared use of facilities. Zia Elementary School in the Highland area provides one example in which the grounds are available as a public park and picnic ground during off-hours. Also, Highland High School in particular maintains a wide range of recreational facilities. Over time the need of the surrounding community will change, especially as it redevelops

into one of Albuquerque's premier urban centers. As such, the needs of the community will need to be evaluated on a continual basis. New community services will need to be provided for according to these needs. It will be important that the City have a system to continually evaluate existing facilities and community services and, as the need arises, implement improvements so that the level of service for the Plan area improves rather than degrades.

5. Streetscapes and Parks

Goal: Create appropriately landscaped and designed streets and parks that foster a beneficial pedestrian and vehicular environment as well as enhance recreational opportunities.

A significant topic of citizen concern in the Plan area is to increase the amount of vegetative cover as a means to reduce heat build-up, enhance the pedestrian experience, and provide public spaces for residents and visitors to relax in the public sphere. Streetscape issues are characterized by a concern with replacing and caring for dying trees (many of which are well-established and contribute greatly to a valued streetscape aesthetic) as well as establishing new corridors for which street trees and other landscaping will greatly enhance non-vehicular mobility. The following map, Streetscapes and Parks, illustrates the location of existing parks as well as medians along Central Avenue with the potential for improved landscaping. However, it also shows proposed sites for public spaces. Additionally, the map also proposes to change Lead, Coal, Zuni and Campus into pedestrian-friendly streets through careful landscaping and design. For Monte Vista, a large, landscaped median is proposed.

Policies

1. The City shall develop landscaped medians to reduce surface runoff and heat island effect.

2. On streets with excess right-of-way (e.g. Monte Vista) larger landscaped medians shall be developed to the above effect as well as provide pedestrian crossing refuges.
3. The City shall replace and replant dying trees in public streetscapes in the Plan area neighborhoods.
4. The City shall develop multi-function public spaces.
5. The city shall improve public streetscapes along major streets in the plan area.

Actions

Where possible, existing medians should be landscaped to reduce surface runoff and mitigate the "heat island" effect. One street, Monte Vista, has a wide right-of-way and as such is well-suited for development with a landscaped median. In addition to improving the environmental conditions for local users, the addition of a median will assist pedestrian crossing by providing a refuge safe from vehicular traffic.

Plan area residents have requested streetscape improvements for Carlisle Boulevard, a major street in the Plan area that does not have wide right-of-way, in order to soften the impact of vehicular traffic on immediately adjacent residential property. Streetscape improvements should also benefit pedestrian users.

Lastly, there has been an expressed need for plazas or other dynamic public spaces that can serve a range of public functions such as celebrations and farmer's markets. This plan identifies two properties (the De Anza Motor Lodge and the Hiland Theater) that are publicly owned and may be well-suited for such development. Both properties are identified as potential "catalyst" nodes within the Central-Highland-Upper Nob Hill Master Plan, making them ideal sites for such redevelopment. Dynamic spaces that can be easily transformed from, say, parking spaces to gathering sites for special events are specifically encouraged.

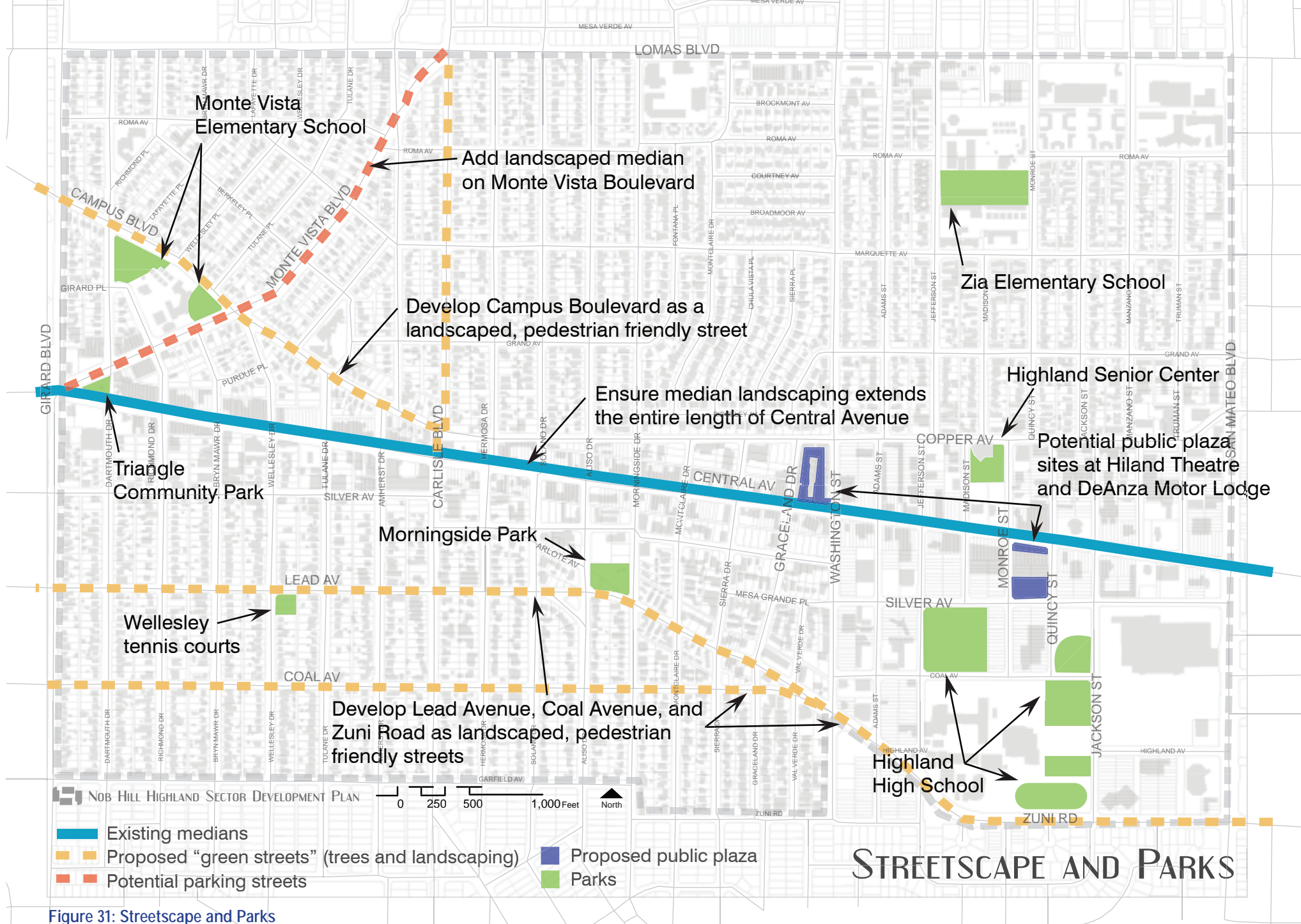


Figure 31: Streetscape and Parks



C. Infrastructure

1. Storm Water Drainage

Goal: Address any storm water drainage issues that might negatively impact the quality of life in the Plan area.

As mentioned in section III, the storm water drainage system in the Nob Hill Highland area relies heavily upon the capacity of the streets, especially major arterials, to handle large flows of storm water. According to the flood insurance rates map on the following page, Central, Copper and Zuni are susceptible to shallow sheet flows. During heavy summer monsoons these shallow flows can be severe as water that extends almost from curb to curb along Central Avenue. One attempt to address this problem was the creation of the Highland detention pond, capable of holding 19 acre feet of water during large storm events. Still, during very heavy rains, the surface runoff can be curb to curb along Central west of Washington even with its 12" high curbs and the Highland detention pond. However, because many of the Nob Hill Highland area's major roads act as storm water runoff channels, any modification to the street section, including bulbouts at corners and at mid-block crossings, will change the capabilities of that street to handle previous runoff amounts creating worse flood conditions. As such, addressing this problem is key to moving forward on numerous recommendations throughout the plan.

Policy

1. The City shall upgrade the storm water drainage system in the Nob Hill Highland area to accommodate proposed changes to the streetscape.

Actions

One proposed solution to the storm water problem was an extension of the Highland detention pond to accommodate a further 16 acre feet bringing the total holding capabilities to 35 acre feet. However, Highland High School rejected this proposal since it would require surrendering land that is currently in use to the flood control project.

Since the extension of the Highland Detention Pond was rejected, the City issued another request for proposals for storm water drainage engineering services that was due May 10, 2006. The request asked respondents to address the entire Campus Wash Drainage Basin from its upstream reaches to where it enters the North Diversion Channel, and address any effect on runoff that road construction might have. Once the City of Albuquerque has chosen a plan, drainage upgrades can proceed.



*Nob Hill Highland
Sector Development Plan*

Flood Insurance Rates

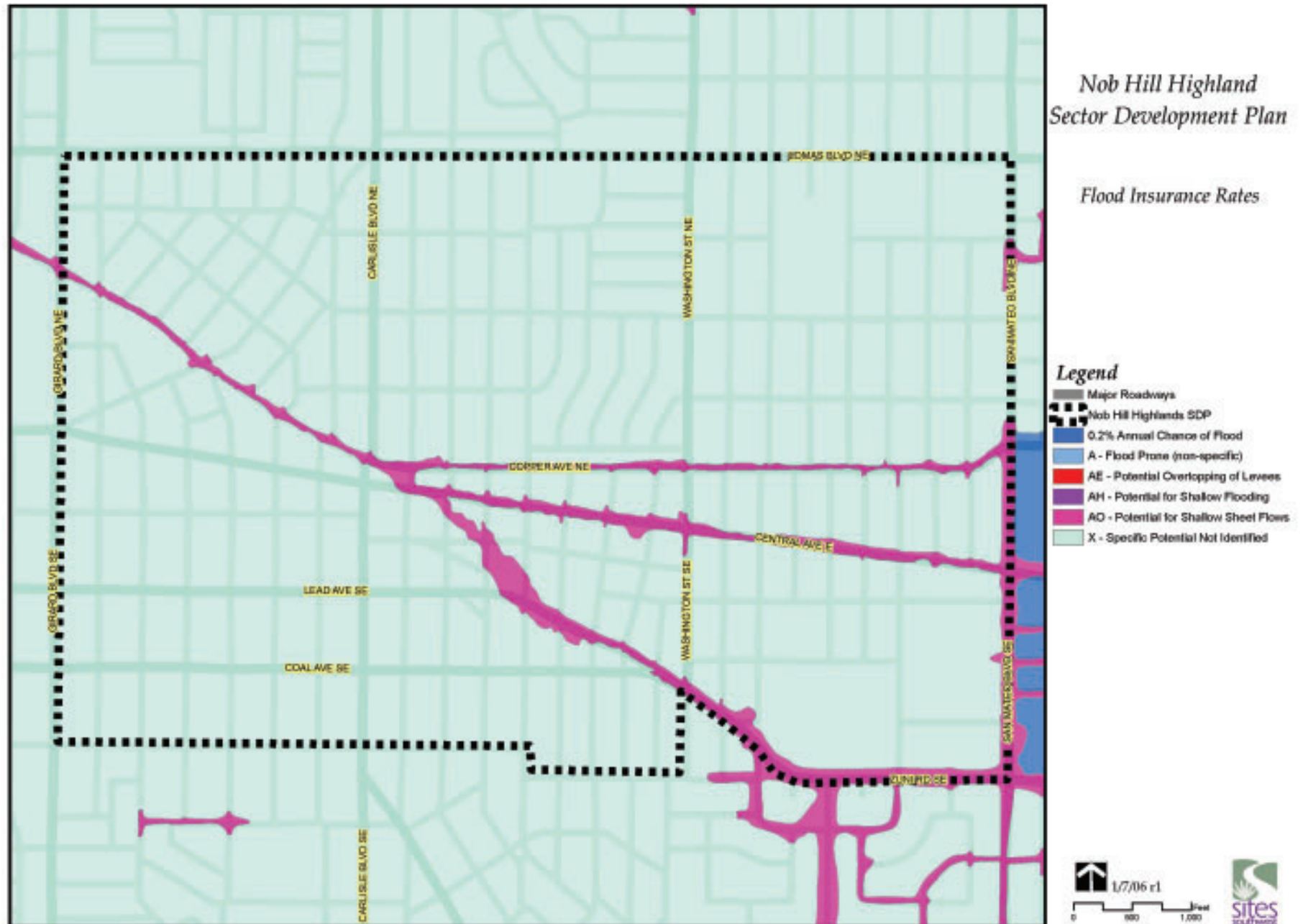


Figure 32: Flood Insurance Rates

D. Economic Vitality

The goal of the plan is to create an environment where businesses, and particularly small, locally owned businesses, can thrive.

Nob Hill today is a vibrant retail center that provides a variety of specialty items and entertainment to neighborhood residents, residents of greater Albuquerque and visitors. East of Carlisle in the Upper Nob Hill and Highland areas, the retail environment is less vibrant, although small businesses of all types are located there.

Policies

1. The City shall work with businesses and neighborhood organizations to improve economic conditions in the plan area.
2. The City shall use its powers under the Metropolitan Redevelopment Act to proactively promote the redevelopment of vacant and underutilized properties.
3. The City shall use financial tools available through the area's designation as a Metropolitan Redevelopment Area, such as tax increment financing and public/private partnerships to accomplish the goals of the plan, including public improvements, affordable housing and commercial revitalization.
4. The City shall work with Bernalillo County to develop county owned land in a way that is congruent with this plan and the County's vision.

Actions

1. **Support Renovation of Historic Buildings for Appropriate Commercial Use** - Underutilized or vacant buildings with historic character provide potential locations for small businesses. Renovating, maintaining

and leasing existing buildings are the responsibility of the property owner and/or the leasing agent. However, the City and organizations such as the Nob Hill Highland Renaissance Corporation, the Nob Hill Business Association and the Highland Business and Neighborhood Association can support the efforts of property owners. The City's zoning regulations should encourage appropriate renovation and maintenance.

2. **Support the Renovation of Other Commercial Buildings and Redevelopment of Vacant and Underutilized Properties for Business Expansion and New Businesses** - The City zoning should make renovation and appropriate new development an attractive option for property owners and businesses seeking to locate in the Nob Hill and Highland Areas.
3. **Provide for a Mix of Uses Along Commercial Corridors** - A mix of land uses provides for both business activity and residential opportunities. This mix, in combination with the established neighborhoods in and adjacent to the plan area, provides an attractive location for businesses that serve residents. Zoning for the commercial corridors through the plan area should support this mix.
4. **Establish a Business Improvement District** - The Nob Hill and Highland areas have begun work towards a Business Improvement District. Such a district would generate funds for ongoing maintenance, marketing, privately funded security, or similar projects. Private property owners must initiate the formation

of a district, and the City should support the Business Improvement District with technical assistance and Council approval of the district.

5. **Provide Financial Support for Capital Improvements Projects and Public/Private Development through MRA Financing Tools** - East of Carlisle, the area's MRA designation allows the City to support private efforts through streetscape improvements, infrastructure improvements, tax incentives and direct participation in public/private projects.
6. **Provide Information About Small Business Development Resources to Nob Hill and Highland Entrepreneurs** - A variety of resources for existing and new small businesses are available in Albuquerque, ranging from the Federal Small Business Administration to Small Business Development Centers located in the metro area. Resources at the Central New Mexico Community College and the Southeast Team for Entrepreneurial Success (STEPS), which operates exclusively in the Southeast Heights, are convenient to businesses located in the sector plan area. Existing organizations, including the Nob Hill Highland Renaissance Corporation, the Nob Hill Business Association, and the Highland Business and Neighborhood Association could make sure that small businesses that are located in the plan area or interested in locating in the plan area have access to this information.
7. **Use MRA Financial Tools to Support the Development of Catalytic Projects within the MR Area Boundary** - The Central



Highland/Upper Nob Hill Master Plan identified two potential catalytic projects for the Metropolitan Redevelopment Area: Redevelopment of the Hiland Theater Area and redevelopment of the De Anza Motel and surrounding area. The De Anza Motel project has been partially implemented through the acquisition and conveyance of the motel property. The plan also identified general public improvements which are included in the movement systems and infrastructure parts of the Sector Plan.

Catalytic projects identified in the MRA plan include the following components:

- **De Anza Motel Area** - The De Anza project was included in the adopted MR plan and is partially implemented by the acquisition and disposition of the motel property. The catalytic project identifies redevelopment of surrounding properties as additional opportunities. Public improvements identified to support this project include:
 - 1) Adoption of mixed use zoning (to be accomplished through the adoption of the Sector Development Plan). The anticipated mix includes small retail, office, live/work units, and other residential development.
 - 2) Pedestrian improvements
 - 3) Parking improvements, including centralized parking courts/structures and parallel and head-in parking along side streets.

Hiland Theater Area

- 1) Adoption of mixed use zoning (to be accomplished through the adoption of the Sector Development Plan). The anticipated mix includes retail, live/work units, apartments, lofts and townhouses.
- 2) Improved pedestrian environment, including traffic calming, median improvements, enlarged sidewalks. Streetscape improvements would include proper lighting, landscape and street furniture.
- 3) Parking improvements, including on-street parking along Central and parallel or head-in parking on side streets. Private parking at the rear of properties or in existing parking courts adjacent to Central.

Additional catalytic projects based on work done for the Sector Plan include:

- **Affordable Housing** - Acquire property for the development of mixed-income housing in the MR Area boundary or include affordable housing within the De Anza and Hiland Theater area projects. Mixed income housing could be in residential-only or mixed-use projects that incorporate retail, office, and residential uses. Both homeownership and rentals are appropriate. The goal is to provide diverse housing opportunities with subsidies and tax credits available to assist qualified low- and moderate-income buyers and renters.

- **Public Plaza** - Construct a public plaza in conjunction with metropolitan redevelopment projects. Potential locations on land in public ownership or within a public/private project are shown in Figure 31.

- **Centralized Public Parking** - The type of development envisioned along Central Avenue is difficult to accomplish without centralized parking facilities. Historically, development in Nob Hill has had less parking on site than is required by the City zoning code. Even with reduced parking requirements for commercial and residential development, it can be difficult to meet parking requirements on a single site without creating a suburban environment. Centralized parking through parking courts and parking structures is desirable. As described in the Movement Systems section of the Sector Development Plan, parking would be public and paid for with a combination of public and private funds.

Implementation

The Plan Implementation section of this document, Section 5, presents a consolidated list of the above recommendations and delineates the City agencies and potential funding sources vital to their realization. Recommendations fall into one of the following categories: Capital Improvements Projects, Program Enhancements, or Community Initiatives, as elaborated in Section 5.A.; or they are established through zoning regulations, as elaborated in Section 5.B.



V. PLAN IMPLEMENTATION

PLAN IMPLEMENTATION / PROJECTS AND PROGRAMS

Abbreviations and Acronyms used in the following table:

APD = Albuquerque Police Department
 APS = Albuquerque Public Schools
 CIP = Capital Improvements Project
 CNM = Central New Mexico Community College
 COA = City of Albuquerque
 DMD = Department of Municipal Development (COA)
 GABAC = Greater Albuquerque Bicycling Advisory Committee
 GARTC = Greater Albuquerque Recreational Trails Committee
 HBNA = Highland Business and Neighborhood Association
 LRTP = Long-range Transportation Plan
 MRA = Metropolitan Redevelopment Agency (COA)

MRCOG = Mid-Region Council of Governments
 NHHRC = Nob Hill Highland Renaissance Corporation
 NHBA = Nob Hill Business Association
 NHNA = Nob Hill Neighborhood Association
 NTMP = Neighborhood Traffic Management Program (COA)
 RFP = Request for proposals
 SBA = Small Business Administration (federal)
 SHPO = State Historic Preservation Office
 STEPS = Southeast Team for Entrepreneurial Success
 UNM = The University of New Mexico

All new and reconstructed sidewalks and streetscapes shall be designed and built in accordance with the best practices described and illustrated in the American Association of State Highway and Transportation Officials (AASHTO) Guide for the Planning, Design, and Operation of Pedestrian Facilities.

A. Projects and Programs

Element	ID	Policy Implemented	Type	Action	Lead Agency	Coordination Required	Potential Funding Sources
A. MOVEMENT SYSTEMS							
Pedestrian/ Bicycle/ Vehicle Circulation	A-1	PC1, PC2, PC3, PC5, BC1, VC1, VC2	Capital Improvement Project	Develop a Streetscape Schematic Design Plan for Central Avenue between Girard and San Mateo Boulevards. Address the following elements: <ul style="list-style-type: none"> - Landscape buffering (shade trees, medians, bulb-outs, planting strips) - Pedestrian improvements (signalized crossings and crosswalks, sidewalks, ADA accessibility, signage alerting motorists of pedestrian and cyclist activity, addition of benches and seating along sidewalks, evaluation of unused curb cuts for closure) - Pedestrian-scale lighting - Traffic calming measures - Lane widths - Transit stops with seating and shade (coordinated with Modern Streetcar plan) - Way-finding - On-street parking - Public art - Impact of Central Blvd. lane reductions on Silver and Copper Aves. between Washington St. and San Mateo Blvd. 	Planning Department (COA)	Parks Management (COA); DMD (COA); Traffic Engineering Division (COA); City Forester (COA)	CIP; Individual projects already funded – see Appendix C



PLAN IMPLEMENTATION / PROJECTS AND PROGRAMS

Element	ID	Policy Implemented	Type	Action	Lead Agency	Coordination Required	Potential Funding Sources
Pedestrian Circulation	A-2	PC1	Capital Improvement Project	Develop safe, visible crosswalks at existing traffic signals on Lead and Coal.	DMD (COA); Traffic Engineering Division (COA)	Lead-Coal Study (DMD, COA)	CIP
Pedestrian Circulation	A-3	PC1, PC3	Capital Improvement Project	Modify Coal and Lead to shorten crosswalks, build level sidewalks, create buffers between sidewalks and vehicles, and slow traffic to improve pedestrian safety, accessibility and comfort.	Traffic Engineering Division (COA); Planning Department (COA)	Lead-Coal Study (Planning Department, COA)	CIP
Bicycle Circulation	A-4	BC3	Capital Improvement Project	Modify Lead and Coal Avenues to allow for installation of protected commuter bike lanes.	DMD (COA); Traffic Engineering Division		CIP
Pedestrian Circulation	A-5	PC4, PC5	Regulation - See Section 5B	Modify zoning to allow future development to orient toward alleyways, but only where street orientation would not be compromised as a result. In these situations, double-fronted buildings are required.	Planning Department (COA)		N/A
Pedestrian Circulation	A-6	PC4	Program Enhancements	Maintain alleys. Residents should identify problem areas through Streets Hotline complaint system.	DMD (COA)	Solid Waste (COA); Residents	General Fund
Pedestrian Circulation	A-7	PC3, PC4	Community Initiative	Identify alleys suitable for public art or community gardens.	Community Organizations; Alley Gardens project	Property Owners; MRA/Planning (COA)	NA
Pedestrian Circulation	A-8	PC3, PC5	Capital Improvement Project	Establish Night Sky Friendly pedestrian-scale lighting on Central, Lead, Coal, Zuni Silver and Washington.	DMD (COA); MRA/Planning (COA)	PNM, Planning Department (COA)	CIP
Pedestrian Circulation	A-9	PC3	Capital Improvement Project	<p>In cooperation with local community organizations, conduct an inventory of all sidewalks and crosswalks in the plan area to identify barriers and other access limitations for people with disabilities.</p> <p>Develop a plan to remediate existing access deficiencies in the walkway network and widen sidewalks to the standards adopted in this plan, with higher priority accorded to safe travel routes along arterial and collector streets, and along local roads in close proximity to schools, bus stops, and other key pedestrian travel destinations. Incorporate into the plan a detailed description of all actions needed to achieve compliance with U.S. Access Board guidelines, a schedule to achieve compliance, a funding strategy, and an assignment of official responsibility for plan implementation.</p>	DMD (COA)	Planning Department (COA); Walk Albuquerque; Alliance for Active Living; UNM	CIP



PLAN IMPLEMENTATION / PROJECTS AND PROGRAMS

Element	ID	Policy Implemented	Type	Action	Lead Agency	Coordination Required	Potential Funding Sources
Pedestrian Circulation	A-10	PC3, safety	Program Enhancement	Identify pedestrian pathways connecting schools, transit and commercial areas and ensure their safety as walking routes, such as the Highland walkway. Coordinate with community organizations for on-going surveys of sidewalks to ensure walkability.	Planning Department (COA); NHNA; NHHRC; NHBA; HBANA	DMD (COA); Walk Albuquerque; Alliance for Active Living; UNM	Safe Routes to School (federal program administered by NM-DOT)
Pedestrian Circulation	A-11	PC3, safety	Program Enhancement	Improve code enforcement of private landscaping to ensure unobstructed sidewalk right-of-way for pedestrian use.	Code Enforcement (COA)	Residents; Neighborhood Associations	General Fund
Pedestrian Circulation	A-12	PC3, safety	Community Initiative	Report traffic problems to improve enforcement of traffic law violations (speeding and failure to stop), especially around schools.	Neighborhood and Business Associations	APD; Traffic Analyses (COA)	
Pedestrian Circulation	A-13	PC5	Community Initiative	Encourage area restaurants to enhance street vitality by providing outdoor seating for patrons, but only in cases where a 5-foot clear passageway for pedestrians can be maintained at all times. Encourage businesses to provide awnings or shade trees over sidewalks to provide shade and relief from heat, subject to the vertical and horizontal clearance guidelines of the U.S. Access Board.	Property Owners	NHRC; NHBA; HBNA; CCR Zoning	
Pedestrian Circulation	A-14	PC5	Program Enhancement	Establish public art as part of all Capital Improvements in the plan area.	DMD/Public Art (COA)	Property Owners; Community Organizations; MRA/Planning (COA)	Public Art Program (COA)
Bicycle Circulation	A-15	BC2, BC3	Capital Improvement Project	Complete the installation of bike lanes and routes in plan area as designated in the Bikeways Master Plan (routes on Copper Ave. and Valverde Dr.; lanes on Carlisle Blvd., Lead Ave., Coal Ave., and Zuni Rd.).	DMD (COA)	Planning Department (COA); MRCOG; LRTP; GARTC; GABAC	CIP
Bicycle Circulation	A-16	BC2	Program Enhancement	Designate Silver Ave. between Washington St. and San Mateo Blvd. as a bike route; remove Central Ave. from Bikeways Master Plan as bike corridor due to safety concerns.	Parks and Recreation (COA)	MRCOG (LRTP process)	General Fund
Bicycle Circulation	A-17	BC1	Community Initiative	Provide public bicycle racks in existing shopping areas.	Property Owners; NHHRC; NHBA; HBNA; NHNA; Community Organizations	Zoning enforcement	
Bicycle Circulation	A-18	BC1	Community Initiative	Promote bicycle commuting and provide all-day bike storage for area employees.	Community Organizations; NHHRC; NHNA; NHBA; HBNA	Property Owners	



PLAN IMPLEMENTATION / PROJECTS AND PROGRAMS

Element	ID	Policy Implemented	Type	Action	Lead Agency	Coordination Required	Potential Funding Sources
Vehicular Circulation	A-19	VC1, PC2	Program Enhancement	Assess the need for traffic calming measures on residential streets throughout plan area to slow through-traffic (i.e., stop signs, bulb outs, diverters, etc.).	DMD (COA); Traffic Engineering Division (COA)	Neighborhood Associations	CIP, NTMP
Transit	A-20	T1, PC5	Regulation – See Section 5B	Establish area around Hiland Theater as a TOD node with high density commercial and residential uses, consistent with Bernalillo County goal for the property. Work with streetscape planning efforts to coordinate adjacent Rapid Ride stop (and Streetcar stop in future phases).	Bernalillo County; ABQ Ride (COA)	ABQ Ride (COA); Planning Department (COA); Modern Streetcar Project	Modern Streetcar Project
Transit	A-21	T1, PC3, PC5	Capital Improvement Projects	Consolidate transit stops where possible (for Rapid Ride and Streetcar). Improve transit stops with shade, seating, and lighting. Coordinate stops with signalized mid-block crossings.	ABQ Ride (COA)	Modern Streetcar Project; Community Organizations	CIP
Transit	A-22	T1, PC3	Program Enhancement	Monitor and participate in HDR design guidelines for modern streetcar transit stop in the Nob Hill Historic Core. Coordinate with signalized mid-block crossings.	ABQ Ride (COA)	Modern Streetcar Project; Community Organizations	Modern Streetcar Project
Parking	A-23	P1	Capital Improvement Projects	Establish feasibility of north/south “parking streets” (such as Amherst St.) through a survey of existing rights-of-way and access requirements. Survey 100-blocks north and south of Central that appear to have sufficient right-of-way to verify right-of-way.	DMD (COA)	Planning Department (COA); Property Owners	CIP
Parking	A-24	P1, P3, CF-V7	Capital Improvement Project	Establish public parking courts/structures to promote ‘park once’ nodes. Wrap structures with office, retail, or residential structures. Create, fund and define a timeline for a public process to identify the location of the first and subsequent public/private funded parking courts/structures.	Planning Department (COA)	DMD (COA); Nob Hill and Highland Neighborhood Associations and Developer Stakeholders	CIP, On-site parking fee for waivers, TIF district
Parking	A-25	P2	Program Enhancement	Promote development to eliminate large, unused parking lots.	Planning Department (COA); MRA/Planning (COA)	Property Owners	General Fund
Parking	A-26	P3	Community Initiative	Consider parking permits in residential areas that restrict parking for residents only between 5 pm and 8 am.	Residents; Community Organizations (to make request)	Parking Division (COA)	General Fund



PLAN IMPLEMENTATION / PROJECTS AND PROGRAMS

Element	ID	Policy Implemented	Type	Action	Lead Agency	Coordination Required	Potential Funding Sources
B. COMMUNITY FORM							
Commercial – District 3	B-1	CF-District 3-1, CF-District 3-2	Program Enhancement	Develop a Corridor Plan for Lomas, including streetscape, land-use, and zoning to accommodate adequate planning for all four corners of the intersection with San Mateo.	Planning Department (COA)		General Fund, CIP
Residential - University Heights & Granada Heights	B-2	CF-University Heights & Grenada Heights-1	Program Enhancement	Survey University Heights and Granada Heights for potential eligibility as historic district and State or National Register nomination of individual properties.	Planning Department (COA)	SHPO	General Fund, SHPO
Residential	B-3	CF-University Heights & Grenada Heights-1; CF-Monte Vista / College View-1, 2	Program Enhancement	Develop and support initiatives that promote awareness and preservation of the historic character and streetscapes of the neighborhood (e.g., a handbook on historic character and guidelines for rehabilitation, development of walking routes with interpretive monuments, and signage which gives identity to historic districts.	Planning Department (COA)	Neighborhood Associations; Community Organizations	General Fund, SHPO
Public Facilities/ Community Services	B-4	PF/CS1	Program Enhancement	Establish a system for periodically meeting with the community and APS to explore ways to share grounds, recreational facilities, and buildings with the broader community.	Planning Department (COA)	APS; Parks & Recreation (COA); Family & Community Services; Community Organizations; Neighborhood Associations	General Fund
Streetscapes and Parks	B-5	S&P-1, S&P-2	Capital Improvement Project	Landscape existing medians throughout the Plan area to prevent run-off, address heat island effect, and assist in safe pedestrian crossing.	DMD (COA)	Parks Management (COA); City Forester (COA)	CIP
Streetscapes and Parks	B-6	S&P-4	Capital Improvement Project	Establish public plazas (possibly at the De Anza Motor Lodge and/or the Hiland Theater).	Property Owners	MRA/Planning (COA); City Forester (COA)	CIP
Streetscapes and Parks	B-7	S&P-5	CIP	Improve streetscape of Carlisle Boulevard to soften impact of traffic on adjacent residential properties and improve pedestrian safety and aesthetic environment.	DMD (COA)	City Forester (COA)	CIP
Streetscapes and Parks	B-8	S&P-6	CIP	Add landscaped median on MonteVista Boulevard, and develop Campus Boulevard as a landscaped, pedestrian friendly street.	DMD (COA)		CIP



PLAN IMPLEMENTATION / PROJECTS AND PROGRAMS

Element	ID	Policy Implemented	Type	Action	Lead Agency	Coordination Required	Potential Funding Sources
C. INFRASTRUCTURE							
Infrastructure	C-1	I1	Capital Improvements Project	Make upgrades to the drainage system in the Campus Wash Drainage Basin, taking into account future road construction in the Plan area and results of the Campus Drainage study, particularly regarding improving the water carrying capacity of Central Avenue with additional inlet infrastructure, as well as changing the flood designation of Central Avenue.	DMD (COA)	Planning Department (COA)	CIP
D. ECONOMIC VITALITY							
Economic Vitality	D-1	EV-2	Program Enhancement/Community Initiative	Establish TIF District to support the redevelopment of the De Anza Motel and other catalytic projects.	Planning Department (COA)	Private Developers	General Fund
Economic Vitality	D-2	EV-1, EV-2	Community Initiative	Create an incentive plan for commercial property owners with a low occupancy rate to encourage leasing of shops to small and independently-owned businesses.	NHHRC; NHBA	Property Owners	
Economic Vitality	D-3	EV-1, EV-2	Community Initiative	Actively recruit new businesses to the area.	NHHRC; HBNA; NHBA		
Economic Vitality	D-4	EV-1, EV-2	Community Initiative	Create a Business Improvement District (BID) in the area, where property owners and businesses share the costs of implementing specific improvements and programs.	Business and property owners		
Economic Vitality	D-5	EV-1, EV-2, EV-3, EV-4	Program Enhancement	Fund land acquisition and public improvements in support of catalytic projects within the Metropolitan Redevelopment area.	MRA/Planning (COA)	NHHRC; NHBA; HBANA	General Fund; TIF district; Grants; State Legislature Appropriations; City Metropolitan Redevelopment Fund
Economic Vitality	D-6	EV-1	Community Initiative	Gather and disseminate information on small business development resources.	NHHRC; HBANA; NHBA	Department of Economic Development (COA); CNM	Federal SBA; STEPS



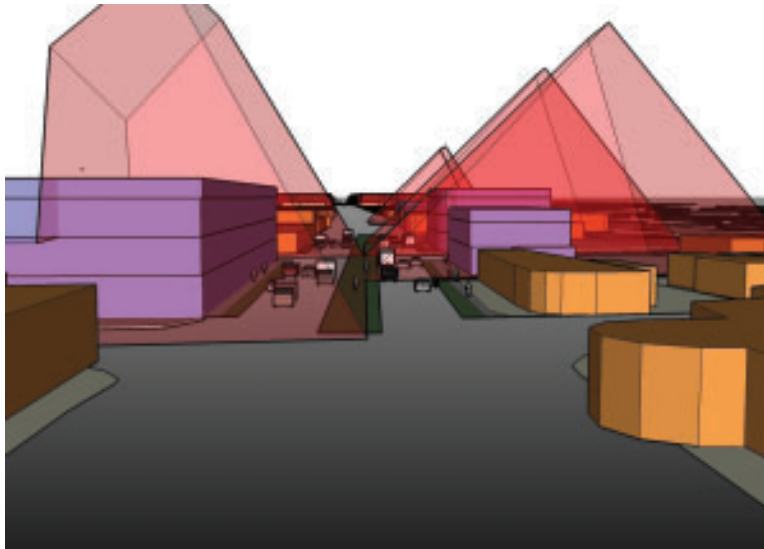
B. Regulations

1. Zoning – Commercial Districts

As a result of the 1987 Nob Hill Sector Plan the primary zoning along Central Avenue in the Historic Core and Emerging Nob Hill districts was changed to CCR (Community Commercial Residential) and OR (Office Residential). The primary purpose of these zone changes was to create the opportunity for mixed residential and commercial use along the corridor and to provide a better transition of uses with surrounding residential areas. Over time the community became interested in modifying the zones to provide more design control and predictability to support their vision for the future of these commercial districts. One of the objectives was to develop better control of building height since the underlying R-2, R-3, C-2 and O-1 zones utilize a 45 degree and 60 degree solar envelope to allow buildings to exceed 26 feet. Analysis of the development potential under

the existing CCR zone, particularly if half or full block developments were to occur, showed the potential for five to seven story building heights in those locations. Another objective was to provide additional design standards to help insure the creation of the future character of the commercial districts as described in the 1987 Nob Hill Sector Plan and the more recent Metropolitan Redevelopment Master Plan for Central – Highland - Upper Nob Hill area. This would include standards governing building height and massing, façade design, building set back and parking area design. The existing commercial zoning in the “Highland” district is primarily C-2 with a small amount of C-1 adjacent to Copper Avenue. This zoning does not allow for the mix of residential and commercial activity that is a major community goal for the Boulevards and Avenues in the Nob Hill Highland Sector Plan Area and that was described in the Metropolitan Redevelopment Master Plan for Central/Highland/Upper Nob Hill area.

The zoning proposed by this Sector Development Plan creates a “form based” version of the CCR zone (CCR-1, CCR-2 and CCR-3) and develops three distinct zoning areas to allow for the variation of requirements for the three commercial districts: Route 66 Historic Core, Emerging Nob Hill and the Highland District. There are two OR zones (OR-1 and OR-2) proposed to allow for the same variation of requirements as you move toward the future metropolitan redevelopment area west of Carlisle. The new zones that are proposed do not change the current permissive uses of the existing zones CCR and OR zones. The proposed CCR-3 does modify the existing C-2 and C-1 zones in that area by the elimination of alcoholic beverage sales for off-premise consumption, drive up windows and outdoor motor vehicle and trailer sales as permissive uses. The new zone does, however, add R-3 permissive uses to the current C-2 and C-1 zoned areas.



A key component of these proposed zoning requirements in the commercial districts is a map that establishes the Allowable Building Heights in the area. (See Figure 36)

The objective of these commercial zoning proposals is to realize the goals of the Nob Hill/Highland community as described in this Plan while at the same time creating opportunities, not hardships, for existing property owners. As stated many times at community meetings the desire is to stimulate private and public investment in this unique area that will build on the success of the Historic Nob Hill area and help the Emerging Nob Hill and Highland areas transition from a suburban auto oriented environment to a walkable, pedestrian oriented urban setting with mixed residential, commercial and entertainment opportunities.

CCR-1, CCR-2, CCR-3 and OR-1, OR-2

All of the properties within the CCR-1,2&3 and the OR-1&2 will be governed by a maximum height requirement that will replace the existing solar envelope regulation and will create a consistency of "street wall" massing in these areas. These heights will be defined by an Allowable Building Heights map.

Avenues and Boulevards

The properties fronting on the "Avenues" (Central, Copper, Silver, Coal) and "Boulevards" (Girard, Monte Vista, Carlisle, Lomas and San Mateo) that are within the CCR zone area will have a specific set of requirements that will be focused on enhancing their role as an "Urban Street". The CCR-1,2 & 3 zones will regulate the location of buildings and parking relative to the street and internal property lines and will require conformance to specific architectural form and design requirements directed at creating an active and pedestrian oriented urban street.

Streets, Drives and Roads

The properties fronting on the "Streets" (Washington, Adams, Jefferson, Madison, Monroe, Quincy, Jackson, Manzano and Truman), the "Drives" (Dartmouth, Richmond, Bryn Mawr, Wellesley, Tulane, Amherst, Hermosa, Solano Aliso, Morningside, Montclair, Sierra, Graceland and Val Verde) as well a small section of Zuni Road that are within the CCR or OR zone areas will have two options within the zone. These areas are important to the creation of the mixed use urban vision of the Sector Plan but in some cases specific properties present unique development challenges due to their size, existing buildings, lack of abutting alleys, etc. This is especially true in the Highland area. In response to this the CCR and OR zones offer two

alternative development options, "Urban Streets" and "Conventional". If a property owner chooses to redevelop his or her property under the "Urban Street" option then they will take advantage of the benefits of lower parking requirements and building setback requirements, etc. that are intended to help the property owner "do the right thing" relative to the goals of the community and of this Plan. If, on the other hand, the property owner decides that they don't want to conform to the "Urban Street" requirements they have the option to develop their property under the "Conventional" option. This means that they will be required to meet the requirements of the traditional C-2, R-2, R-3 and O-1 zones, as defined in the City of Albuquerque Zoning Code, with the exception of those requirements governing use and height which has been redefined by the CCR-1,2,3 and OR-1,2 zones.

The following are more detailed descriptions of elements of the proposed Commercial District Zones.

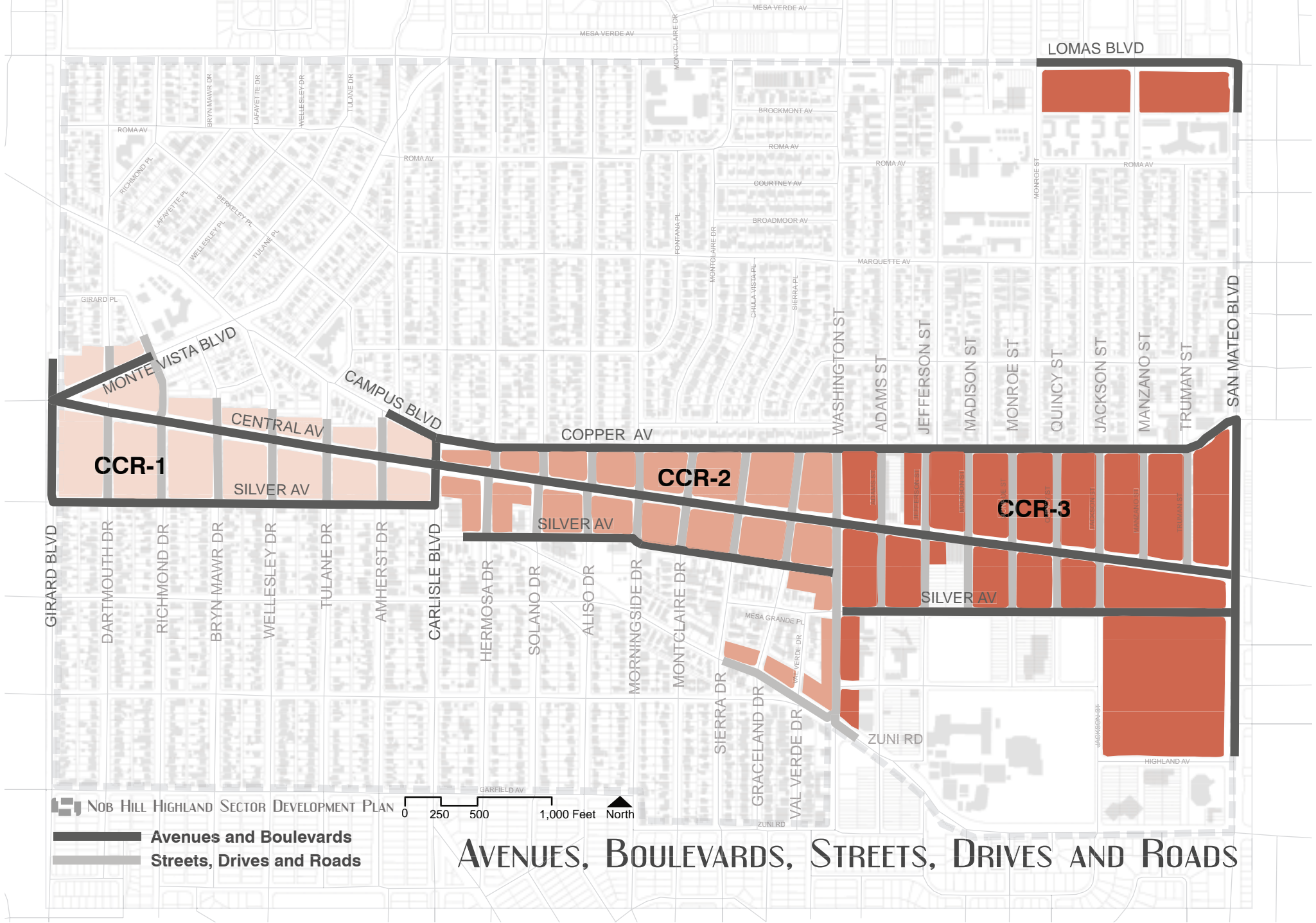
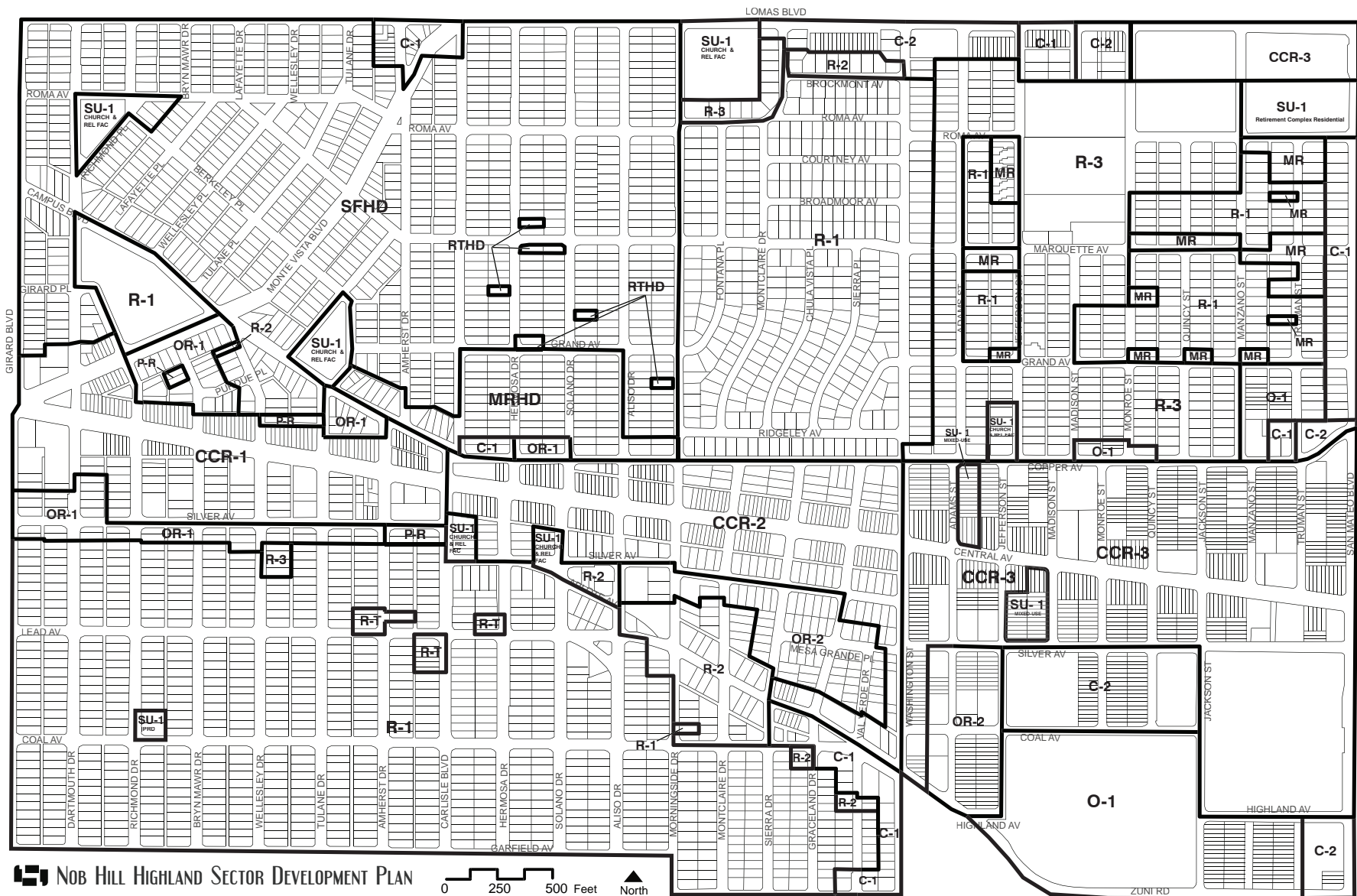


Figure 33: Avenues, Boulevards, Streets, Drives and Roads



- C-1 Neighborhood Commercial
- C-2 Community Commercial
- CCR-1 Community Commercial / Residential 1
- CCR-2 Community Commercial / Residential 2
- CCR-3 Community Commercial / Residential 3
- MR Mixed-Use Residential

- MRHD Mixed-Use Residential Historic District
- O-1 Office and Institutional
- OR-1 Office / Residential 1
- OR-2 Office / Residential 2
- P-R Reserve Parking
- R-1 Single Family

- R-2 Multi-Family
- R-3 Multi-Family
- R-T Residential Townhouse
- RTHD Residential Townhouse Historic District
- SFHD Single Family Historic District
- SU-1 Special Use

COMPREHENSIVE ZONING

Figure 34: Comprehensive Zoning



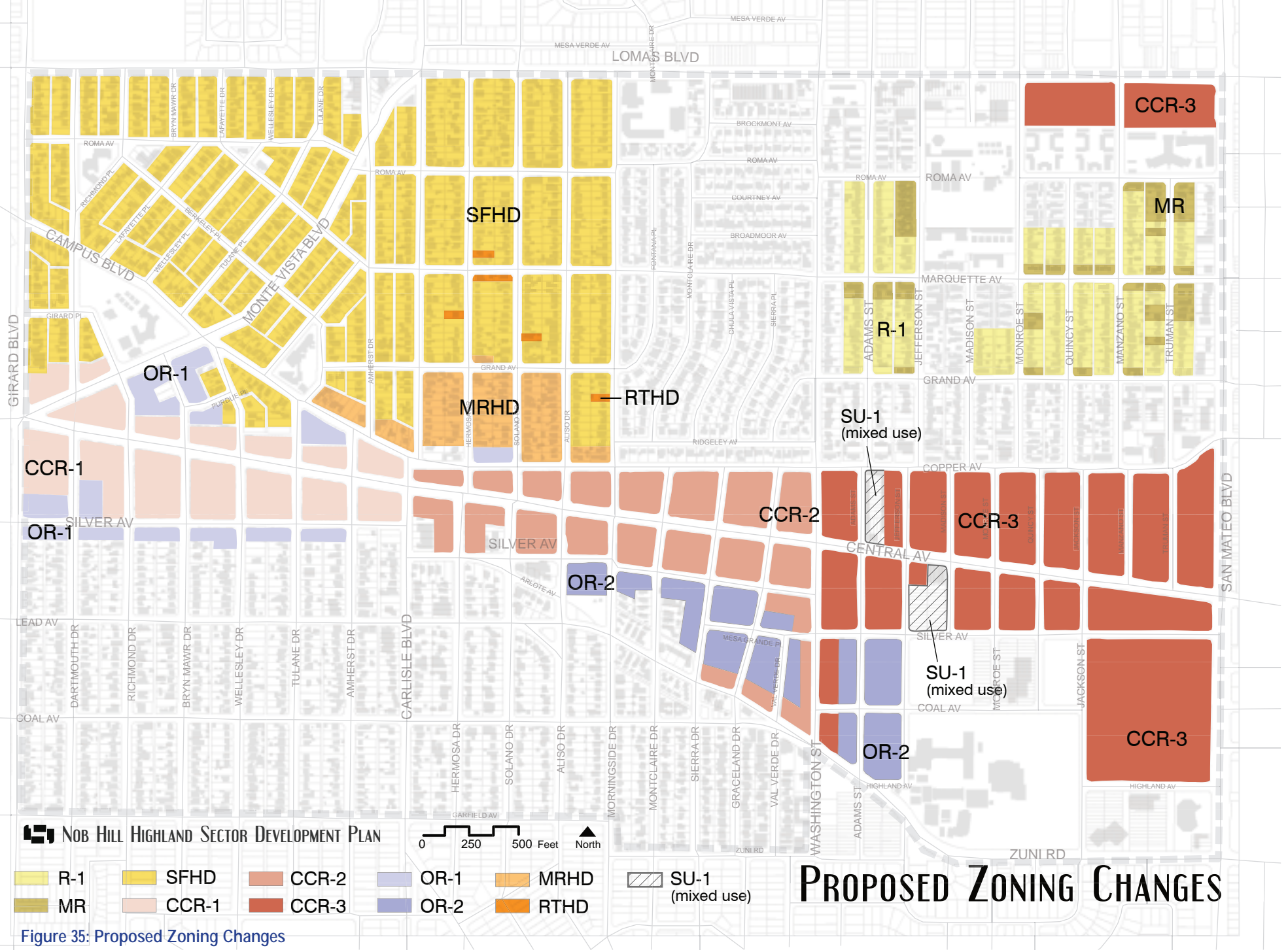


Figure 35: Proposed Zoning Changes

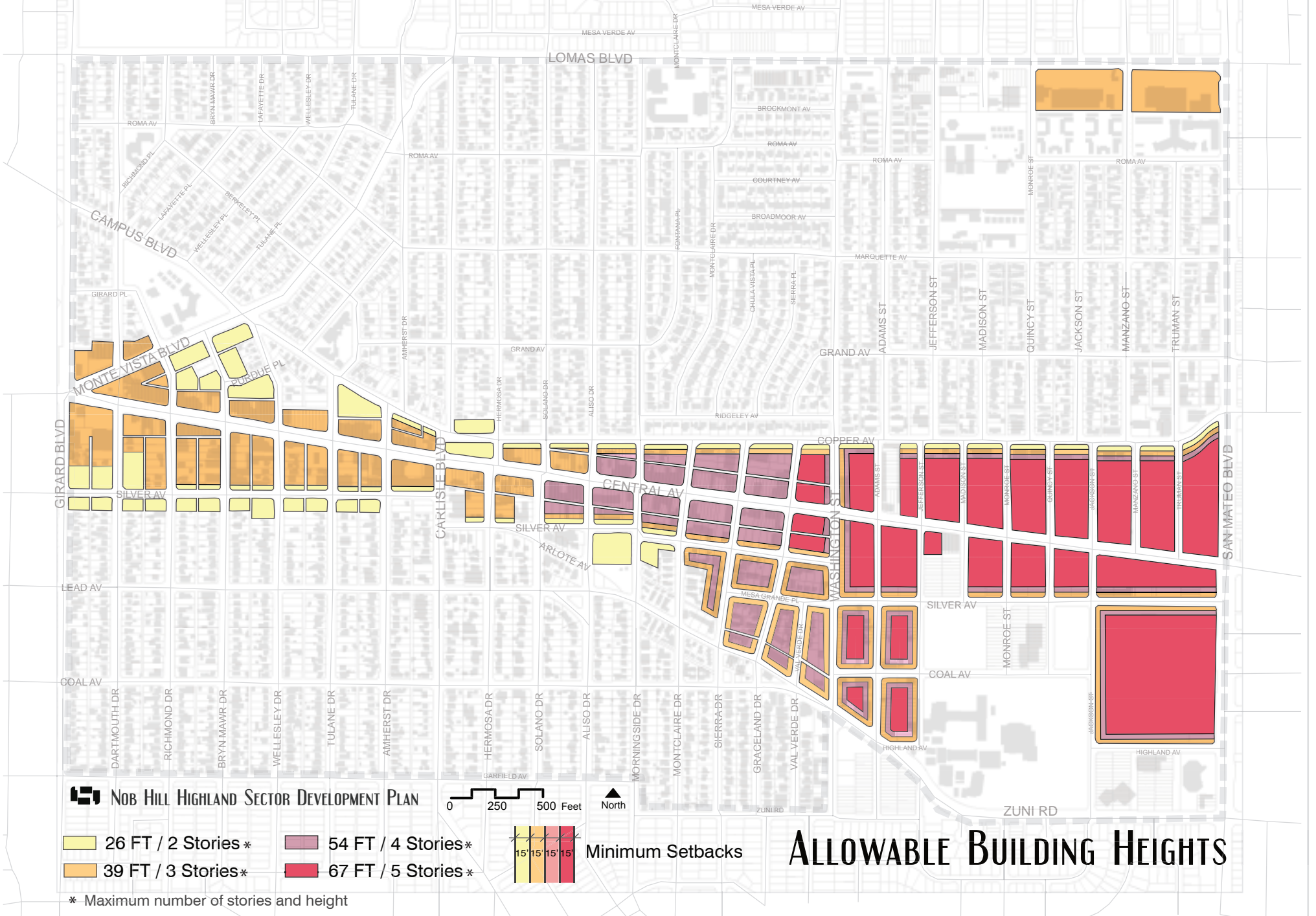


Figure 36: Allowable Building Heights

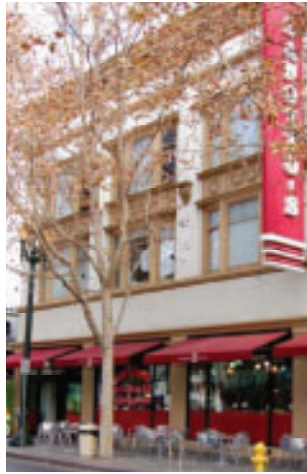


Community Commercial/Residential One - CCR-1

1. General Rules: Uses

The Community Commercial/Residential One (CCR-1) land use category corresponds to the R-3 Zone for residential uses and the C-2 zone for non-residential uses with the following exceptions:

- A. Alcoholic beverage sales for off-premise consumption are not allowed.
- B. Drive-up service windows are not allowed.
- C. Outdoor motor vehicle or trailer sales, retail, service, repair, and/or storage shall be separated from Central Avenue by a building. For the purpose of the CCR Land Use Category, "building" means a structure enclosed on all side by walls and roofs.
- D. There is no maximum floor area ratio or density limit for R-3 residential uses.

**2. Architecture: Form & Elements**

- A. Existing facades that abut a public right-of-way for buildings that are designated on State and National Historic Registers or as City Landmarks or Characteristic Buildings in Figure 3 shall comply with the following:
 1. Façade rehabilitation or remodeling shall maintain the historically characteristic window and door proportion and character of the building.
 2. Any new materials shall be consistent with historically employed materials on such facades.

* Fenestration (Def.) - "The arrangement, proportioning of doors and windows in a building"

** Façade (also Frontage): The exterior wall of a building set parallel to a lot line bordering a public right-of-way or clearly visible from a public right-of-way. An architectural element is not clearly visible if it is visible only through an opening in a street or an opening in a courtyard wall.

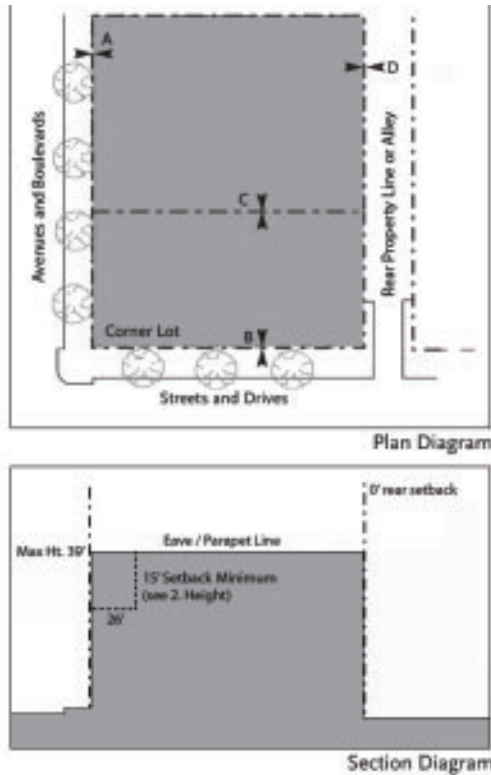
3. Additions to contributing buildings shall reflect the historic façade demarcations (i.e., walls, pilasters or similar elements.)
 4. Additions above contributing buildings shall be vertically aligned with the historic demising walls of the storefronts below (see photo p. 90)
 5. Colors of paint or stucco may be modified without a limit to frequency.
- B. The ground stories facing Central Avenue shall be built to appear as shopfronts and have front fenestration* of 60% minimum of façade**, with window sills no higher than 30 inches above the finished floor.
 - C. Regarding the ground stories facing intersecting side streets to a depth of 150' or to an alley or street, whichever occurs first, when the building or buildings facing Central at a minimum length along Central of 50' are redeveloped or rebuilt, the 40' along the side street from Central shall have at least one entrance and the remaining portion of the side street frontage shall be built to appear as shopfronts or residential facades and shall be consistent in design and materials as to the major facades facing Central. Where it is impractical, as determined by the Planning Director, to locate parking ingress and egress from the alley: (a) one ingress no more than 12' in width is allowed from the side street in situations where egress from the alley is practical; or (b) one ingress / egress location no more than 30' in width is allowed to the side street should option "a" be infeasible. Where storefronts or residences due to the adjacent sidewalk grades make pedestrian access infeasible, as determined by the Planning Director, this requirement may be waived. The side street facade shall have fenestration of 60% minimum of façade** with window sills no higher than 30 inches above the finished floor.
 - D. All other ground floor and upper level building frontage shall be designed with 40-90% of the building frontage glazed, with the window sill no higher than 30 inches above the finished floor.
 - E. Building frontages on alleys shall have ground floor windows no higher than 60" above finished grade at building and shall be designed with 40%-90% of the building frontage glazed. All public alleys shall include pedestrian scale lighting and be maintained free of pedestrian and vehicular obstacles.
 - F. Window glass shall be clear with light transmission between exterior and interior:
 1. 90% minimum for the ground story (within nine feet of the sidewalk elevation); and
 2. 75% minimum for the upper stories (modification permitted as necessary to meet any applicable building and energy code requirements).
 - G. Façade fenestration design shall maintain the established window and door proportions of the block.
 - H. Placement- openings shall occur along street façades at twenty-five feet (25') on center minimum; openings to be three foot (3') wide minimum.
 - I. Entrance - each ground floor building shall have 1 entrance minimum for each twenty five feet (25') or less of building frontage length on street frontage.
 - J. Articulation - building façade at front and side street shall change each twenty five (25') minimum in height, or setback, or material.
 - K. Awnings, arcades and balconies are permitted.
 - L. Property walls & fences - as allowed in current City regulations at fronts, sides & rears of buildings.
 - M. Where setback buildings currently exist the streetwall shall be strengthened through use of solid vertical landscaping and low walls.

3. Signage and Lighting

(See General Regulations page)



Building Placement and Height



4. Setbacks

- A. Buildings shall be placed within the shaded area as shown in the above diagram.
1. Front Build to Line: 0" to twelve inches (12")
 2. Side Street Setback: 0"
 3. Side yard Setback: 0"
 4. Rear Setback: 0"

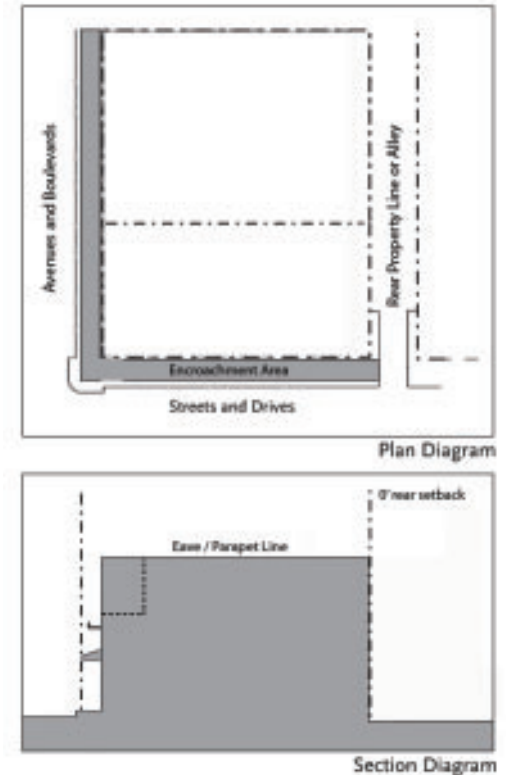
- B. Notwithstanding the other provisions of this Plan, two forecourts are allowed on each side of Central (within the reach of the Route 66 Historic Core and within the reach of Emerging Nob Hill), setback to the following specifications: between ten feet (10') and twenty feet (20') deep from the right-of-way, between twenty-five feet (25') and seventy-five feet (75') wide parallel to the right-of-way, and sited not less than fifty feet (50') from any block corner.

5. Height

Building height shall be measured in feet from grade as defined in the zoning code to top of parapet or midpoint of pitch.

- A. Maximum: 3 stories/39' transition to 26', see Allowable Building Heights Plan. If 75% or more of the block frontage along Central is being developed or redeveloped, one third of the new development is limited to 2 stories (26 feet) in height.
- B. Decorative variations in the parapet height are allowed up to an additional 5%; and
- C. Architectural features, such as a towers, are allowed up an additional 15% height (limited to 5% of building foot print) above the decorative variations in parapet height.

Building Frontage and Profile



6. Encroachments Allowed

Arcades, balconies and awnings may encroach over the Public Right of Way [ROW] at streets. Encroachments into public ROW shall follow existing COA regulations.

7. Frontage Types

Arcade, shop front and forecourt (see Frontage Types Page 104). Not less than 50% of the building facades shall be accompanied by arcades, cantilevered shed roofs, or awnings.

Parking Placement



8. Parking Requirements

- A. On-site parking is allowed only in the shaded areas as shown.
 - 1. Front Setback: 30% of lot depth minimum or 50', whichever is less. Parking lots and off street parking shall be behind or within a building or buildings adjacent to the fronting street.
 - 2. Side Street Setback: six foot (6') minimum with exception of parking completely below grade whose setback is 0" minimum
 - 3. Side Setback: 0" minimum
 - 4. Rear Setback: five foot (5') minimum
- B. Parking requirements below are subject to reductions due to credits allowed by the zone code:
 - 1. 1 space/bedroom, 2 spaces maximum/DU
 - 2. 3 parking spaces: commercial or retail per 1000 s.f.
 - 3. 1 parking space: restaurants, per each 4 seats
 - 4. 1 parking space: per hotel room

- C. Commercial uses 3,000 square feet or less in size are exempt from parking requirements.
- D. Vehicular access is permitted only from side street or alley. Parking garages shall have liner buildings along all side street frontages and solid 3'-0" minimum high walls [all levels] or solid landscape at side and rear property lines; however, if the side or rear property line is adjacent to a residentially-zoned lot, the wall of the parking structure must be entirely solid, without opening.
- E. Surface parking areas at rears of properties shall be shielded from streets by wall, fence, or solid, vertical landscaping. Screening shall be a minimum of 30 inches in height but shall not exceed 36 inches in height.

9. Landscape Requirements

The landscape design should reflect the more urban character of this area in its programming, detailing, and planting intensity. 10% of parking lot areas shall be planted with irrigated shade producing trees. All side street setback areas shall be landscaped to screen parking areas. All properties must comply with Section 14-16-3-10, Landscaping Regulations, of the Zoning Code.

Option: Urban Street/Conventional

10. Urban Street

- A. All properties fronting on an *avenue* or a *boulevard* in this zone will be required to conform to these "urban street" regulations.
- B. Properties fronting on a *drive* or a *street* have the option of developing under these urban street regulations or choosing the conventional option.

11. Conventional Option

- A. The conventional option means that the project will be required to meet the requirements of the traditional C-2, R-2, R-3 and O-1 zones, as defined in the City of Albuquerque Zoning Code, with the exception of use and height which has been redefined by this CCR zone (or OR Zone depending on which zone is applicable).

Community Commercial/Residential Two - CCR-2

1. General Rules: Uses

The Community Commercial/Residential Two (CCR-2) land use category

corresponds to the R-3 Zone for residential uses and the C-2 zone for non-residential uses with the following exceptions:

- A. Alcoholic beverage sales for off-premise consumption are not allowed.
- B. Drive-up service windows are not allowed.
- C. Outdoor motor vehicle or trailer sales, retail, service, repair, and/or storage shall be separated from Central Avenue by a building. For the purpose of the CCR Land Use Category, "building" means a structure enclosed on all side by walls and roofs.
- D. There is no minimum floor ratio or density limit for R-3 residential uses.



4. Additions above contributing buildings shall be vertically aligned with the historic demising walls of the storefronts below (see photo p. 93)
5. Colors of paint or stucco may be modified without a limit to frequency.
- B. The ground stories facing Central Avenue shall be built to appear as shopfronts and have front fenestration* of 60% minimum of facade**, with window sills no higher than 30 inches above the finished floor.
- C. Regarding the ground stories facing intersecting side streets to a depth of 150' or to an alley or street, whichever occurs first, when the building or buildings facing Central at a minimum length along Central of 50' are redeveloped or rebuilt, the 40' along the side street from Central shall have at least one entrance and the remaining portion of the side street frontage shall be built to appear as shopfronts or residential facades and shall be consistent in design and materials as to the major facades facing Central. Where it is impractical, as determined by the Planning Director, to locate parking ingress and egress from the alley: (a) one ingress no more than 12' in width is allowed from the side street in situations where egress from the alley is practical; or (b) one ingress / egress location no more than 30' in width is allowed to the side street should option "a" be infeasible. Where storefronts or residences due to the adjacent sidewalk grades make pedestrian access infeasible, as determined by the Planning Director, this requirement may be waived. The side street facade shall have fenestration of 60% minimum of facade** with window sills no higher than 30 inches above the finished floor.
- D. All other ground floor and upper level building frontage shall be designed with 40-90% of the building frontage glazed, with the window sill no higher than 30 inches above the finished floor.
- E. Building frontages on alleys shall have ground floor windows no higher than 60" above finished grade at building and shall be designed with 40%-90% of the building frontage glazed. All public alleys shall include pedestrian scale lighting and be maintained free of pedestrian and vehicular obstacles.
- F. Window glass shall be clear with light transmission between exterior and interior:
 1. 90 % minimum for the ground story (within nine feet of the sidewalk elevation); and
 2. 75% minimum for the upper stories (modification permitted as necessary to meet any applicable building and energy code requirements).
- G. Placement- openings shall occur along street façades at twenty-five feet (25') on center minimum; openings to be three foot (3') wide minimum.
- H. Entrance - each ground floor building shall have 1 entrance minimum for each twenty five feet (25') or less of building frontage length on street frontage.
- I. Articulation - building façade at front and side street shall change each twenty five feet (25') minimum in height, or setback, or material.
- J. Awnings, arcades and balconies are permitted.
- K. Property walls & fences - as allowed in current City regulations at fronts, sides & rears of buildings.
- L. Where setback buildings currently exist the streetwall shall be strengthened through use of solid, vertical landscaping and low walls

2. Architecture: Form & Elements

- A. Existing facades that abut a public right-of-way for buildings that are designated on State and National Historic Registers or as City Landmarks or Characteristic Buildings in Figure 3 shall comply with the following:
 1. Façade rehabilitation or remodeling shall maintain the historically characteristic window and door proportion and character of the building.
 2. Any new materials shall be consistent with historically employed materials on such facades.
 3. Additions to contributing buildings shall reflect the historic façade demarcations (i.e., walls, pilasters or similar elements.)

* Fenestration (Def.) - "The arrangement, proportioning of doors and windows in a building"

** Façade (also Frontage): The exterior wall of a building set parallel to a lot line bordering a public right-of-way or clearly visible from a public right-of-way. An architectural element is not clearly visible if it is visible only through an opening in a street or an opening in a courtyard wall.

3. Signage and Lighting

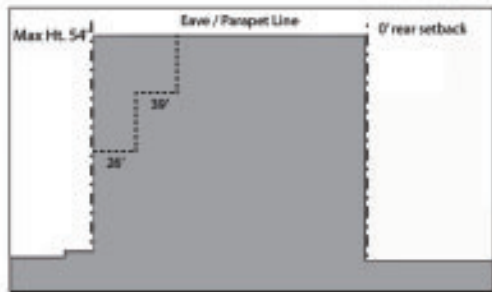
(See General Regulations page)



Building Placement and Height



Plan Diagram



Section Diagram

4. Setbacks

- A. Buildings shall be placed within the shaded area as shown in the above diagram.
1. Front build to line: 0" to twelve inches (12")
 2. Side Street Setback: 0"
 3. Side yard Setback: 0"
 4. Rear Setback: 0"

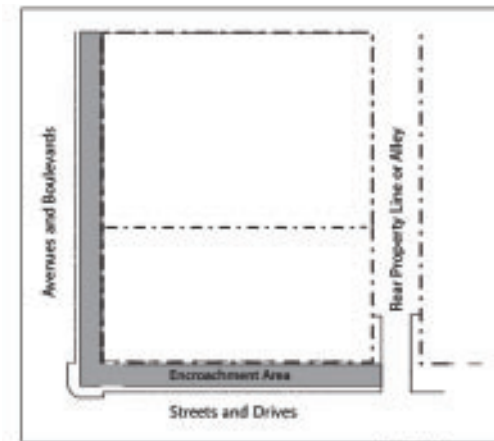
- B. Notwithstanding the other provisions of this Plan, two forecourts are allowed on each side of Central (within the reach of the Route 66 Historic Core and within the reach of Emerging Nob Hill), setback to the following specifications: between ten feet (10') and twenty feet (20') deep from the right-of-way, between twenty-five feet (25') and seventy-five feet (75') wide parallel to the right-of-way, and sited not less than fifty feet (50') from any block corner.

5. Height

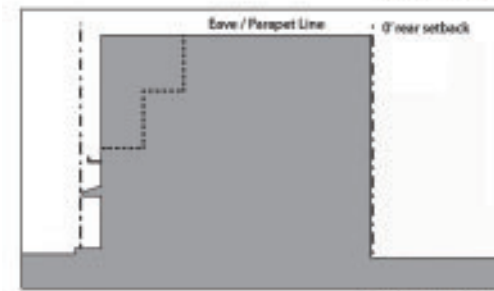
Building height shall be measured in feet from grade as defined in the zoning code to top of parapet or midpoint of pitch.

- A. Maximum: 4 stories/54' with lower heights, see Allowable Building Heights Plan. If 75% or more of the block footage along Central is being developed or redeveloped, twenty-two percent (22%) of the new development is limited to 3 stories (39 feet) in height. No elevation of a building across Central Avenue frontage shall be more than 2 stories (26 feet) greater than an adjoining building (measured from adjacent building not from grade of adjacent lot or sidewalk).
- B. Decorative variations in the parapet height are allowed up to an additional 5%; and
- C. Architectural features, such as a tower, are allowed up an additional 15% height (limited to 5% of building footprint) above the decorative variations in parapet height.

Building Frontage and Profile



Plan Diagram



Section Diagram

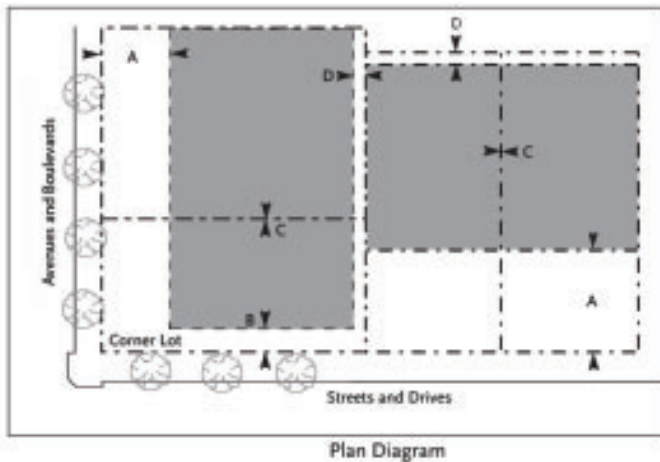
6. Encroachments Allowed

Arcades, balconies and awnings may encroach over the Public Right of Way [ROW] at side streets. Encroachments into public ROW shall follow existing COA regulations.

7. Frontage Types

Arcade, shop front and forecourt (see Frontage Types Page 104). Not less than 50% of the building facades shall be accompanied by arcades, cantilevered shed roofs, or awnings.

Parking Placement



8. Parking Requirements

- A. On-site parking is allowed only in the shaded areas as shown.
1. Front Setback: 30% of lot depth minimum or 50' whichever is less if parking is located completely behind the structure. If parking is located within the structure or if a parking structure is wrapped, the Front Setback shall be 20% of lot depth minimum or 25 feet whichever is less provided that the front of building contains active uses visible from the front and side street(s) if on a corner parcel(s) including retail, office, residential, entertainment, and civic. Parking lots and off street parking shall be behind or within a building or buildings adjacent to the fronting street.
 2. Side Street Setback: six foot (6') minimum with exception of parking completely below grade whose setback is 0" minimum
 3. Side Setback: 0" minimum
 4. Rear Setback: five foot (5') minimum

- B. Parking requirements below are subject to reductions due to credits allowed by the zone code:
1. 1 space/bedroom, 1.5 spaces per 2-bedroom unit 2 spaces maximum/DU
 2. 3 parking spaces: commercial or retail per 1000 s.f.
 3. 1 parking space: restaurants, per each 4 seats
 4. 1 parking space: per hotel room
- C. Commercial uses 3,000 square feet or less in size are exempt from parking requirements.
- D. Uses within 650 feet of a Bus Rapid Transit or Streetcar stop may factor a 20% reduction in parking requirements. Uses from 650 feet to 1300 feet of a Bus Rapid Transit or Street car stop or within 650 feet of another bus stop may factor a 10% reduction in parking requirements. Routes lengths that have bus shelters at all stops may factor in an additional 5% reduction in parking requirements.
- E. Vehicular access is permitted only from side street or alley. Parking garages shall have liner buildings along all side street frontages and solid 3'-0" minimum high walls [all levels] or solid landscape at side and rear property lines; however, if the side or rear property line is adjacent to a residentially-zoned lot, the wall of the parking structure must be entirely solid, without opening.
- F. Surface parking areas at rears of properties shall be shielded from streets by wall, fence, or solid, vertical landscaping. Screening shall be a minimum of 30 inches in height but shall not exceed 36 inches in height.

9. Landscape Requirements

The landscape design should reflect the more urban character of this area in its programming, detailing, and planting intensity. 10% of parking lot areas shall be planted with irrigated shade producing trees. All side street setback areas shall be landscaped to screen parking areas. All properties must comply with Section 14-16-3-10, Landscaping Regulations, of the Zoning Code.

Option: Urban Street/Conventional

10. Urban Street

- A. All properties fronting on an *avenue* or a *boulevard* in this zone will be required to conform to these urban street regulations.
- B. Properties fronting on a *drive* or a *street* have the option of developing under these urban street regulations or choosing the conventional option.

11. Conventional Option

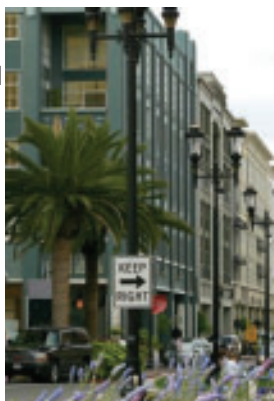
- A. The conventional option means that the project will be required to meet the requirements of the traditional C-2, R-2, R-3 and O-1 zones, as defined in the City of Albuquerque Zoning Code, with the exception of use and height which has been redefined by this CCR zone (or OR Zone depending on which zone is applicable).

Community Commercial/Residential Three - CCR-3

1. General Rules: Uses

The Community Commercial/Residential Three (CCR-3) land use category corresponds to the R-3 Zone for residential uses and the C-2 zone for non-residential uses with the following exceptions:

- A. Outdoor motor vehicle or trailer sales, retail, service, repair, and/or storage shall be separated from Central Avenue by a building. For the purpose of the CCR Land Use Category, "building" means a structure enclosed on all side by walls and roofs.
- B. There is no maximum floor area ratio or density limit for R-3 residential uses.

**2. Architecture: Form & Elements**

- A. Existing facades that abut a public right-of-way for buildings that are designated on State and National Historic Registers or as City Landmarks or Characteristic Buildings in Figure 3 shall comply with the following:
 - 1. Façade rehabilitation or remodeling shall maintain the historically characteristic window and door proportion and character of the building.
 - 2. Any new materials shall be consistent with historically employed materials on such facades.
 - 3. Additions to contributing buildings shall reflect the historic façade demarcations (i.e., walls, pilasters or similar elements.)

* Fenestration (Def.) - "The arrangement, proportioning of doors and windows in a building"

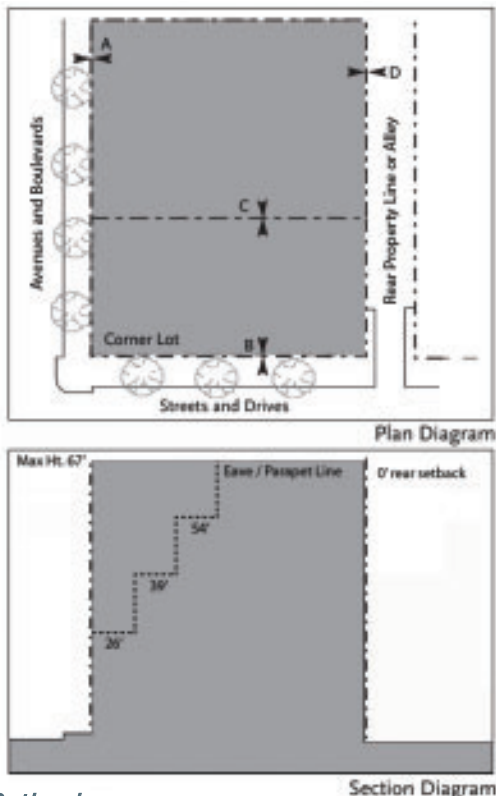
** Façade (also Frontage): The exterior wall of a building set parallel to a lot line bordering a public right-of-way or clearly visible from a public right-of way. An architectural element is not clearly visible if it is visible only through an opening in a street or an opening in a courtyard wall.

- 4. Additions above contributing buildings shall be vertically aligned with the historic demising walls of the storefronts below (see photo p. 96)
- 5. Colors of paint or stucco may be modified without a limit to frequency.
- B. The ground stories facing Central Avenue shall be built to appear as shopfronts and have front fenestration* of not less than 60% minimum of facade**, with window sills no higher than 30 inches above the finished floor.
- C. All other ground floor upper level building frontage shall be designed with 40-90% of the building frontage glazed, with the window sill no higher than 30 inches above the finished floor.
- D. Window glass shall be clear with light transmission between exterior and interior:
 - 1. 90% minimum for the ground story (within nine feet of the sidewalk elevation); and
 - 2. 75% minimum for the upper stories (modification permitted as necessary to meet any applicable building and energy code requirements).
- E. Placement- openings shall occur along street façades at forty feet (40') on center minimum; openings to be three foot (3') wide minimum. Entrance - each ground floor use shall have 1 entrance minimum for each forty feet (40') or less of building frontage length on street frontage.
- F. Articulation - building façade at front and side street shall change each forty feet (40') minimum in height, or setback, or material. Awnings, arcades and balconies are permitted.
- G. Property walls & fences - as allowed in current City regulations at fronts, sides & rears of buildings.
- H. Drive-thru type buildings allowed only in rears of properties or on sides of properties provided that the service window and any associated order board are located at least fifty feet (50') from any residential zone and screened by a structure or wall at least six feet (6') in height.
- I. Where setback buildings currently exist the streetwall shall be strengthened through use of solid, vertical landscaping and low walls

3. Signage and Lighting

(See General Regulations page)

Building Placement and Height



4. Setbacks

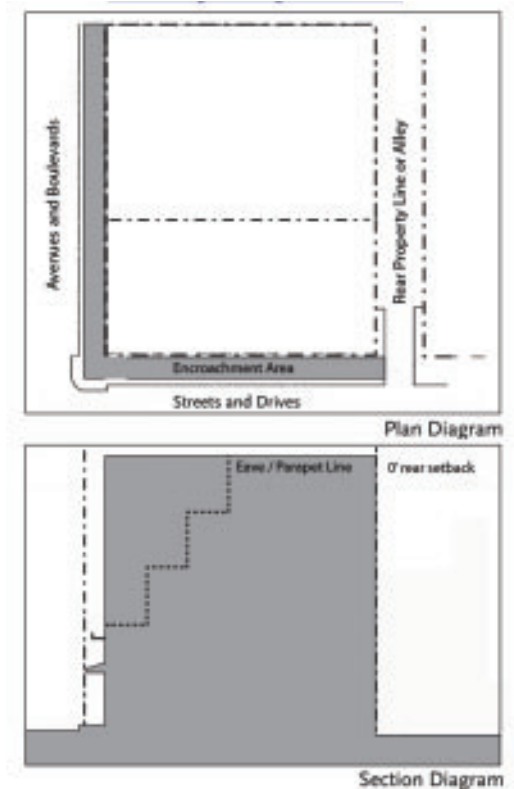
- A. Buildings shall be placed within the shaded area as shown in the above diagram.
1. Front build to line: 0" to twelve inches (12")
 2. Side Street Setback: 0"
 3. Side yard Setback: 0"
 4. Rear Setback: 0"

5. Height

Building height shall be measured in feet from grade as defined in the zoning code to top of parapet or midpoint of pitch.

- A. Maximum: 5 stories/67' and transitions to lower heights, see Allowable Building Heights Plan. If 70% or more of the block frontage along Central is being developed or redeveloped, at least 20% must be five stories (67 feet) in height and 40% of the new development is limited to 4 stories (53 feet) in height.
- B. Decorative variations in the parapet height are allowed up to an additional 5%; and
- C. Architectural features, such as a tower, are allowed up an additional 15% height (limited to 5% of building footprint) above the decorative variations in parapet height and are limited to 5% of floor area.

Building Frontage and Profile

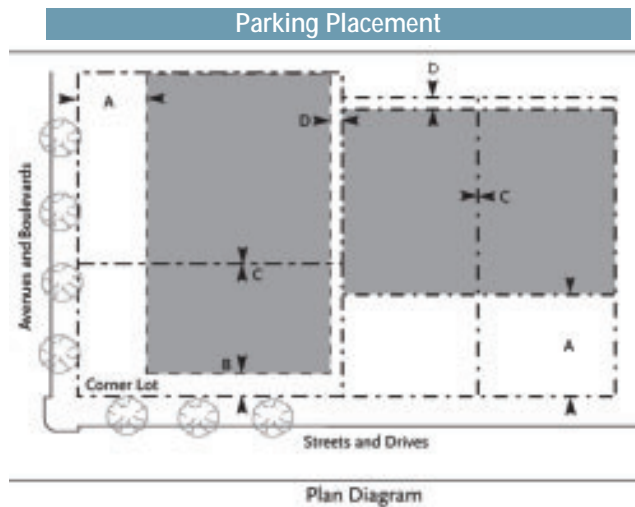


6. Encroachments Allowed

Arcades, awnings and balconies may encroach over the Public Right of Way [ROW] at side streets. Encroachments into public ROW shall follow existing COA regulations.

7. Frontage Types

Arcade, shop front and forecourt (see Frontage Types Page 104). Not less than 50% of the building facades shall be accompanied by arcades, cantilevered shed roofs, or awnings.



8. Parking Requirements

- A. On-site parking is allowed only in the shaded areas as shown.
1. Front Setback: 30% of lot depth minimum or 50', whichever is less if parking is located completely behind the structure. If parking is located within the structure or if a parking structure is wrapped, the Front Setback shall be 20% of lot depth minimum or 25 feet whichever is less provided that the front of building contains active uses visible from the front and side street(s) if on a corner parcel(s) including retail, office, residential, entertainment, and civic. Parking lots and off street parking shall be behind or within a building or buildings adjacent to the fronting street.
 2. Side Street Setback: six foot (6') minimum with exception of parking completely below grade whose setback is 0" minimum
 3. Side Setback: 0" minimum
 4. Rear Setback: five foot (5') minimum

- B. Parking requirements below are subject to reductions due to credits allowed by the zone code:
1. 1.5 spaces per 2-bedroom unit, 1 space/bedroom, 2 spaces maximum/DU
 2. 3 parking spaces: commercial or retail per 1000 s.f.
 3. 1 parking space: restaurants, per each 4 seats
 4. 1 parking space: per hotel room
- C. Uses within 650 feet of a Bus Rapid Transit or Streetcar stop may factor a 20% reduction in parking requirements. Uses from 650 feet to 1300 feet of a Bus Rapid Transit or Street car stop or within 650 feet of another bus stop may factor a 10% reduction in parking requirements. Route lengths that have bus shelters at all stops may factor in an additional 5% reduction in parking requirements.
- D. Vehicular access is permitted only from side street or alley on block frontages of 300 feet or less. Blocks longer than 300 feet may have one access point from fronting street. Parking garages shall have liner buildings along all side street frontages and solid 3'-0" minimum high walls [all levels] or solid landscape at side and rear property lines; however, if the side or rear property line is adjacent to a residentially-zoned lot, the wall of the parking structure must be entirely solid, without opening.
- E. Surface parking areas at rears of properties shall be shielded from streets by wall, fence, or solid, vertical landscaping. Screening shall be a minimum of 30 inches in height but shall not exceed 36 inches in height.

9. Landscape Requirements

The landscape design should reflect the more urban character of this area in its programming, detailing, and planting intensity. 10% of parking lot areas shall be planted with irrigated shade producing trees. All side street setback areas shall be landscaped to screen parking areas. All properties must comply with Section 14-16-3-10, Landscaping Regulations, of the Zoning Code.

Option: Urban Street/Conventional

10. Urban Street

- A. All properties fronting on an *avenue* or a *boulevard* in this zone will be required to conform to these urban street regulations.
- B. Properties fronting on a *drive* or a *street* have the option of developing under these urban street regulations or choosing the conventional option.

11. Conventional Option

The conventional option means that they will be required to meet the requirements of the traditional C-2, R-2, R-3 and O-1 zones, as defined in the City of Albuquerque Zoning Code, with the exception of use and height which has been redefined by this CCR zone (or OR Zone depending on which zone it is in).

Office/Residential One - OR-1

**1. General Rules: Uses**

The OR Office/Residential One (OR-1) land use category corresponds to the R-2 Zone for residential uses and the O-1 Zone for non-residential uses:

- A. Conditional Uses
 - 1. Uses listed as permissive in the R-C Zone; floor area requirements do not apply.
 - 2. Existing non-conforming uses are to be treated as approved conditional uses.
- B. Drive-thru building type not allowed in this category.
- C. There is no maximum floor area ratio or density limit for R-2 residential uses.

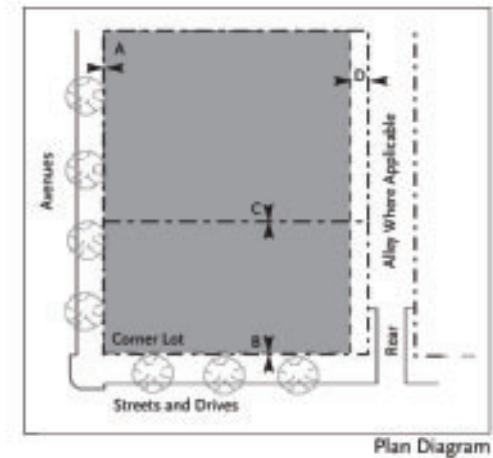
2. Architecture: Form & Elements

- A. Placement- openings shall occur along street façades at thirty feet (30') on center minimum; openings to be three feet (3') wide minimum.
- B. Ground floor building frontage shall be designed with 30-70% of the building frontage length glazed, with the window sill no higher than thirty inches (30") above the finished floor.
- C. Entrance - There shall be at least 1 functioning entry door for every thirty feet (30') of building frontage length on street frontages.
- D. Articulation - building façade at front and side street shall change each twenty five feet (25') minimum in height, or setback, or material.
- E. Property walls & fences - as allowed in current City regulations at fronts, sides & rears of buildings.

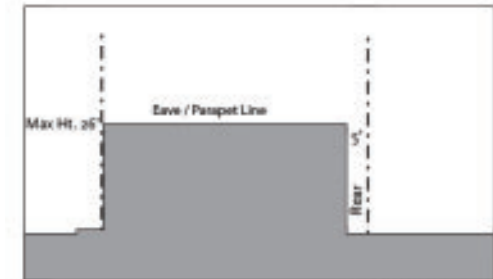
3. Signs

According to general sign regulations in the COA Zoning Code.

Building Placement and Height



Plan Diagram



Section Diagram

4. Setbacks

- A. Buildings shall be placed within the shaded area as shown in the above diagram.
 - 1. Front Build to Line: 0' - 8' per lot
 - 2. Side Street Setback: five feet (5')
 - 3. Side yard Setback: 0'
 - 4. Rear Setback: five feet (5')

5. Height

Building height shall be measured in feet from grade as defined in the zoning code to top of parapet or midpoint of pitch.

- A. Maximum: 2 stories/twenty six feet (26') high maximum, see Allowable Building Heights Plan.

Building Frontage and Profile

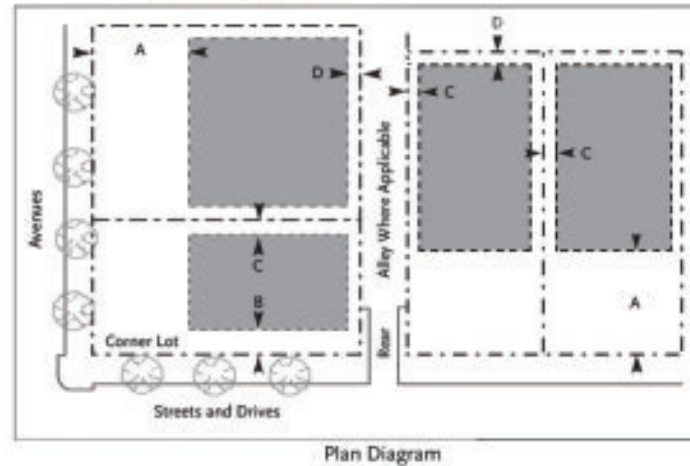
6. Encroachments Allowed

No encroachments allowed.

7. Frontage Types

Arcade, shop front and forecourt (see Frontage Types Page 104). Not less than 50% of the building facades shall be accompanied by arcades, cantilevered shed roofs, or awnings.

Parking Placement

**8. Parking Requirements**

- A. On-site parking is allowed only in the shaded areas as shown above and defined in the text below.
 1. Front Setback: 30% of lot depth minimum, unless below grade. Parking lots and off street parking shall be behind or within a building or buildings adjacent to the fronting street.
 2. Side Street Setback: six foot (6') minimum with exception of parking completely below grade whose setback is 0" minimum
 3. Side yard Setback: five feet (5') minimum
 4. Rear Setback: five feet (5') minimum
- B. Parking requirements below are subject to reductions due to credits allowed by the zone code:
 1. Office: 3 per 1,000 square feet
 2. Residential: 1 space/bedroom, 2 spaces maximum.
- C. Vehicular access is permitted only from the side streets and from an alley.

- D. Existing garages at rears of properties of existing access drives from streets are permitted.
- E. Surface parking areas at rears of properties shall be shielded from streets by wall, fence, or solid, vertical landscaping. Screening shall be a minimum of 30 inches in height but shall not exceed 36 inches in height.

9. Landscape Requirements

The landscape design should reflect the more urban character of this area in its programming, detailing, and planting intensity. 10% of parking lot areas shall be planted with irrigated shade producing trees. All side street setback areas shall be landscaped to screen parking areas. All properties must comply with Section 14-16-3-10, Landscaping Regulations, of the Zoning Code.

Option: Urban Street/Conventional

10. Urban Street

All properties fronting on an *avenue* or a *boulevard* in this zone will be required to conform to these urban street regulations.

Properties fronting on a *drive* or a *street* have the option of developing under these urban street regulations or choosing the conventional option.

11. Conventional Option

The conventional option means that the project will be required to meet the requirements of the traditional C-2, R-2, R-3 and O-1 zones, as defined in the City of Albuquerque Zoning Code, with the exception of use and height which has been redefined by this OR zone (or CCR Zone depending on which zone is applicable).

Office/Residential Two - OR-2



1. General Rules: Uses

The OR Office/Residential land use category corresponds to the R-2 Zone for residential uses and the O-1 Zone for non-residential uses:

- A. Conditional Uses
 - 1. Uses listed as permissive in the R-C Zone; floor area requirements do not apply.
 - 2. Existing non-conforming uses are to be treated as approved conditional uses.
- B. Drive-thru type building type not allowed in this category.
- C. There is no maximum floor area ratio or density limit for R-2 residential uses.

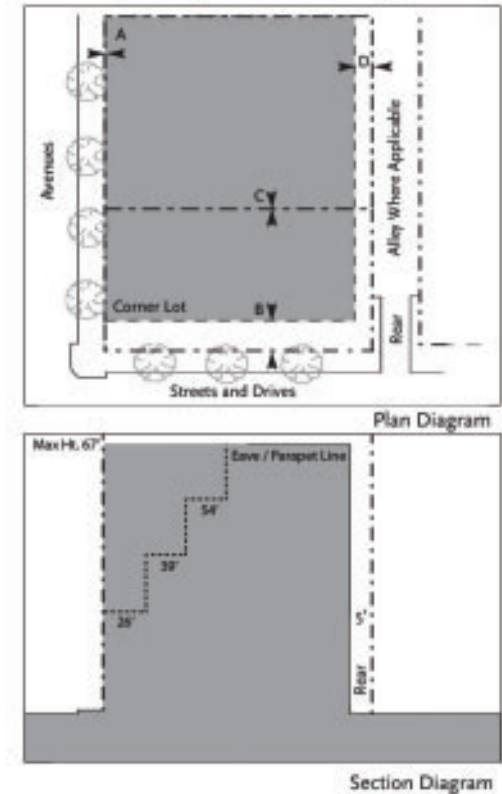
2. Architecture: Form & Elements

- A. Placement- openings shall occur along street façades at thirty feet (30') on center minimum; openings to be three feet (3') wide minimum.
- B. Ground floor building frontage shall be designed with 30-70% of the building frontage length glazed, with the window sill no higher than thirty inches (30") above the finished floor.
- C. Entrance - There shall be at least 1 functioning entry door for every thirty feet (30') of building frontage length on street frontages.
- D. Articulation - building façade at front and side street shall change each fifty feet (50') minimum in height, or setback, or material.
- E. Property walls & fences - as allowed in current City regulations at fronts, sides & rears of buildings.

3. Signs

According to general sign regulations in the zoning code.

Building Placement and Height



4. Setbacks

- A. Buildings shall be placed within the shaded area as shown in the above diagram.
 - 1. Front Build to Line: 0' - 8' per lot
 - 2. Side Street Setback – Adjacent to Surface Parking Only: 10'
 - 3. Side yard Setback: 0'
 - 4. Rear Setback: five feet (5')

5. Height

Building height shall be measured in feet from grade as defined in the zoning code [on site] to top of parapet or midpoint of pitch.

- A. Maximum: 3 stories/thirty-nine feet (39') high to 5 stories/sixty-seven (67') high, see Allowable Building Heights plan.

Building Frontage and Profile

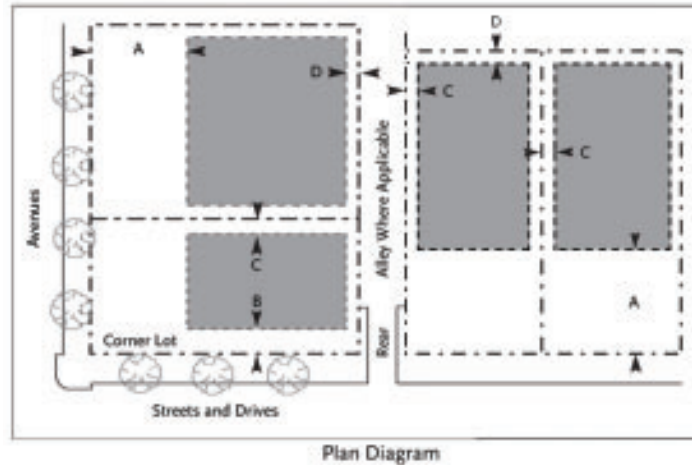
6. Encroachments Allowed

No encroachments allowed.

7. Frontage Types

Arcade, shop front and forecourt (see Frontage Types Page 104). Not less than 50% of the building facades shall be accompanied by arcades, cantilevered shed roofs, or awnings.

C. Parking Placement



Plan Diagram

8. Parking Requirements

- A. On-site parking is allowed only in the shaded areas as shown above and defined in the text below.
 1. Front Setback: 30% of lot depth minimum, unless below grade. Parking lots and off street parking shall be behind or within a building or buildings adjacent to the fronting street.
 2. Side Street Setback: six foot (6') minimum with exception of parking completely below grade whose setback is 0" minimum
 3. Side yard Setback: five feet (5') minimum
 4. Rear Setback: five feet (5') minimum
- B. Parking requirements below are subject to reductions due to credits allowed by the zone code:
 1. Office: 3 per 1,000 square feet
 2. Residential: 1 space/bedroom, 2 spaces maximum/DU
- C. Vehicular access is permitted only from sides streets and the alley. Parking garages shall have liner buildings at all street frontages and solid 3'-0" minimum high walls [all levels] or solid landscape at side and rear property lines;

provided, however, if the side or rear property line is adjacent to a residentially-zoned lot, the wall of the parking structure must be entirely solid, without opening.

- D. Existing garages at rears of properties of existing access drives from streets are permitted.
- E. Surface parking areas at rears of properties shall be shielded from streets by wall, fence, or solid, vertical landscaping. Screening shall be a minimum of 30 inches in height but shall not exceed 36 inches in height.

9. Landscape Requirements

The landscape design should reflect the more urban character of this area in its programming, detailing, and planting intensity. 10% of parking lot areas shall be planted with irrigated shade producing trees. All side street setback areas shall be landscaped to screen parking areas. All properties must comply with Section 14-16-3-10, Landscaping Regulations, of the Zoning Code.

Option: Urban Street/Conventional

10. Urban Street

- A. All properties fronting on an *avenue* or a *boulevard* in this zone will be required to conform to these urban street regulations.
- B. Properties fronting on a *drive* or a *street* have the option of developing under these urban street regulations or choosing the "conventional" option.

11. Conventional Option

The conventional option means that the project will be required to meet the requirements of the traditional C-2, R-2, R-3 and O-1 zones, as defined in the City of Albuquerque Zoning Code, with the exception of use and height which has been redefined by this OR zone (or CCR Zone depending on which zone is applicable).

General Regulations

1. Signage

- A. One projecting sign, not to project more than 30" may be allowed for each business provided:
 - 1. Sign is separated by at least 25' from any other projecting sign, and
 - 2. There is not another sign for the same business on the premises.
 - 3. Sign heights shall not exceed 5' above the height of a building [parapet or midpoint of pitched roof].
- B. Wall signs shall not exceed 20% of a building front's façade area.
- C. Illumination, motion: in addition all regulations in the City's sign code [Section 14-16-3-5 ROA 1994], change of illumination may produce apparent motion of the visual image on signs.
- D. Allowable types of signage: 3-dimensional, blade, or projecting signs, wall-mounted signs w/ recessed or raised letters; letters [individual] of metal or light box construction; masonry or metal plaques; street address numbers; awning signage - cloth, metal, or glass, shall be open at sides & bottom [no boxed or 1/4 round shapes]; neon; window paint - names or logos may be painted on the street level windows. Signage details:
 - 1. Individual letters shall not exceed 18 inches in height or width and 3 inches in relief.
 - 2. Painted window signs shall fit in 8 sq. ft. area or less.
 - 3. Masonry or bronze plaques shall fit in an 18 sq. ft. area or less.

2. Lighting

- A. Storefronts - street level storefronts shall be lit @ the exterior w/ building-mounted fixtures [in compliance with City Dark Sky standards] during dark hours of the day.
- B. Building sides & rears - if lit, may use floods or directional lights and must be shielded or aimed to prevent view of lamp filament/source from areas off of the property.
- C. Lighting Lamp elements: shall be incandescent, metal halide, or halogen only. HID or fluorescent not allowed [except compact fluorescents which screw directly into standard sockets].
- D. Public Streets - in addition to City standard lighting for traffic, sidewalk areas lighting shall be pedestrian-scaled lamp on shaft/base, w/ head @ 12' to 15' high, funded as Public infrastructure.
- E. All light fixtures shall be in compliance with Section 14-16-3-9 ROA 1994, Area Lighting Regulations of the Comprehensive City Zoning Code.

3. Historic Preservation

- A. All buildings on the State and National Historic Registers, City Landmarks, and Characteristic Buildings shall be preserved within the project area. Alteration, demolition, and/or new construction to buildings on the State and National Historic Registers, City Landmarks and Characteristic Buildings in Figure 3 shall be reviewed by the LUCC staff and a report provided in a timely way to the Development Review Division of the Planning Department. LUCC staff shall proceed to have designated Characteristic Buildings, the Nob Hill Business Center, Monte Vista Fire Station, De Anza Motor Lodge, and Solar Building as City Landmarks.

4. Environmental Planning Commission Review

- A. Sites larger than 5 acres require review and approval from the Environmental Planning Commission. Those sites over 5 acres in size at the adoption of this plan shall comply with this standard regardless of platting action.

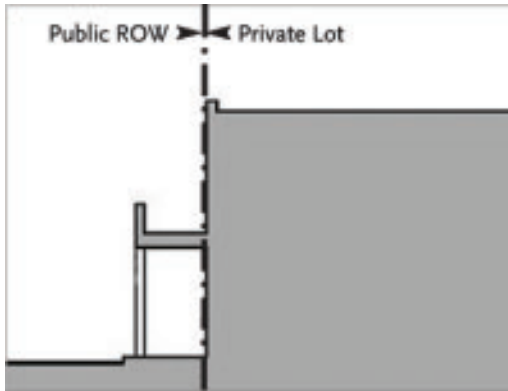


5. Frontage Types

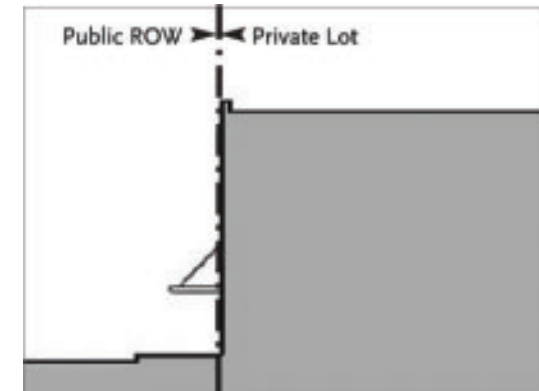
Frontage Types are applied to each zone. These represent a range of additions to the basic facade of the building, in the following illustrations, "ROW" means the public street right-of-way.

Allowed Frontage Types

Type	CCR-1	CCR-2	CCR-3	OR-1	OR-2
Arcade	★	★	★		
Shop Front	★	★	★	★	★
Stoop				★	★
Forecourt	★	★	★		

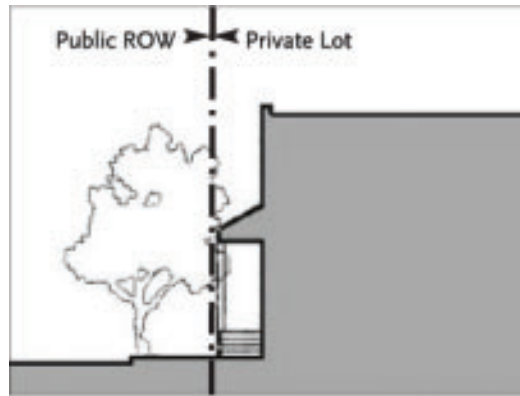


A. Arcade
The facade of a building with an attached colonnade. Balconies may overlap the sidewalk while the ground floor remains set at the lot line. This type is ideal for retail use, but only when the sidewalk is fully absorbed within the arcade so that a pedestrian cannot bypass it. An easement for private use of the right-of-way is usually required. To be useful, the arcade should be no less than 8 feet wide clear in all directions.



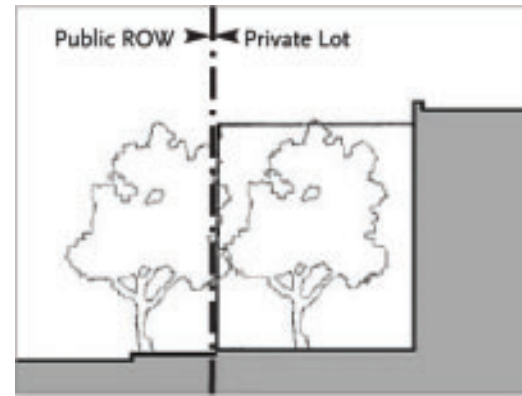
B. Shop front
The facade is placed at or close to the right-of-way line, with the entrance at sidewalk grade. This type is conventional for retail frontage. It is commonly equipped with cantilevered shed roof or awning. The absence of a raised ground floor story precludes residential use on the ground floor facing the street, although this use is appropriate behind and above.





C. Stoop

The facade is placed close to the frontage line with the ground story elevated from the sidewalk, securing privacy for the windows. This type is suitable for ground floor residential uses at short setbacks. This type may be interspersed with the shop front. A porch may also cover the stoop.



D. Forecourt

The facade is aligned close to the frontage line with a portion of it setback. The resulting forecourt is suitable for gardens, vehicular drop offs, and outdoor dining. This type should be used sparingly and in conjunction with the shop fronts. A fence or wall at the property line may be used to define the private space of the yard. The court may also be raised from the sidewalk, creating a small retaining wall at the property line with entry steps to the court.



2. Zoning – Residential Districts

Monte Vista/College View Historic District

Issues and Analysis

The Monte Vista and College View Historic District has distinctive characteristics in architectural styles, subdivision pattern and streetscape. Preservation of the district's historic character is a primary goal of this plan. Of particular interest to the residents of the neighborhood are scale and massing of buildings, which have historically been one-story with uniform front yard setbacks, varying by block from 20 feet up to 40 feet.

The historic architectural styles in the district, with distinctive details and materials, height and massing, and the historic streetscape are important features to preserve. Individual freedom to renovate and upgrade the homes over time is also important to the community, as long as the overall character of the neighborhood is maintained. Height, massing and streetscape are regulated through zoning. The approach to architectural styles, materials and other details is education and voluntary guidelines.

Solar access is a primary concern. Special neighborhood zoning incorporates the City's existing Building Height Limitations to Preserve Solar Access for all lots regardless of the age of the subdivision.

The properties that are proposed for rezoning are shown on the enlarged segment of the Zoning Map.

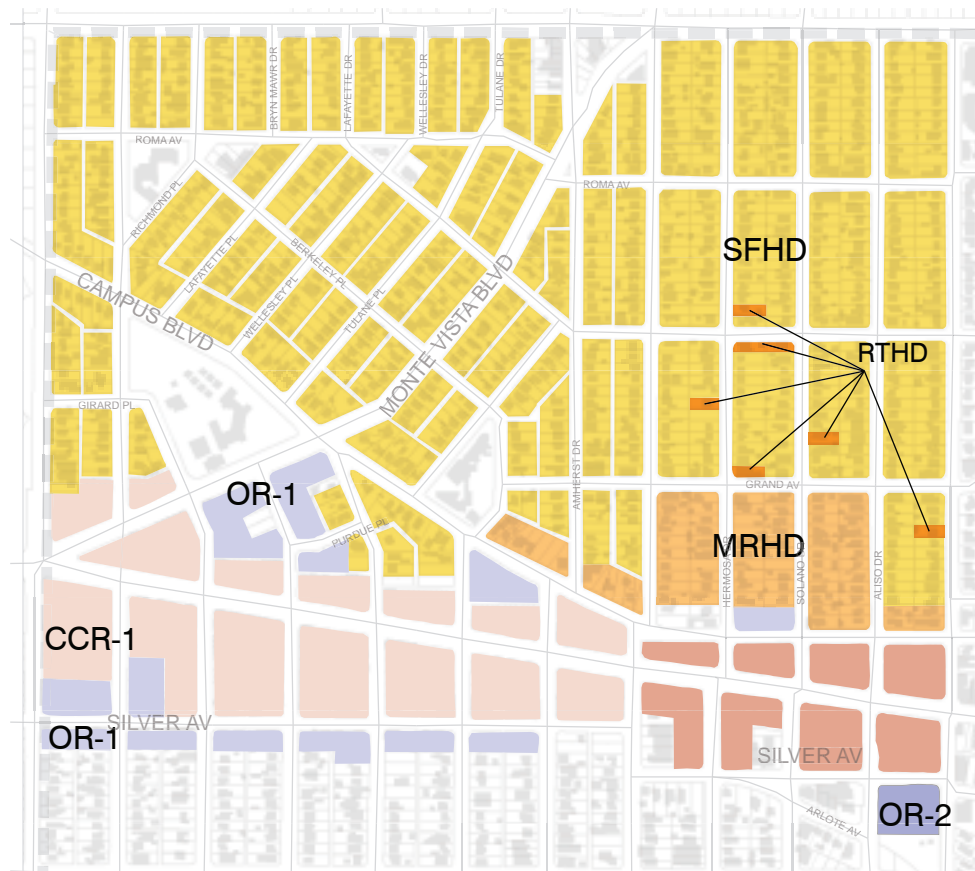
Zoning Proposals

The plan designates the residential portion of the historic district as part of the SU-2 Special Neighborhood Zone for the plan area. This zone is based upon the existing residential zones with minor modifications. The new zone will

- Preserve the historic streetscape;
- Preserve the scale of buildings from the street; and
- Preserve solar access to properties, while allowing homeowners the flexibility to renovate and expand their homes.

Zoning Districts

New SU-2 zoning districts are added to the Sector Development Plan zone map for properties within the Monte Vista and College Heights Historic District and zoned R-1, R-T or MR prior to the adoption of this plan.



SU-2/SFHD

The SU-2/SFHD Single Family Historic District zone corresponds to the R-1 Zone in the Zoning Code with the following exceptions:

A. Conditional Uses

1. A carport within the front yard setback is not allowed.
2. Walls, fences and retaining walls greater than three feet in height are not allowed in the front yard setback.
3. Existing nonconforming uses are to be treated as approved conditional uses.

B. Height

1. Buildings shall comply with the Building Height Limitations to Preserve Solar Access, §14-16-3-3 (A) (7) of the Zoning Code. The additional height limitations of this section shall apply to any lot regardless of the age of the building, except that structures existing as of the date of adoption of the Plan are allowed subject to Zoning Code regulations for buildings that are nonconforming as to height.
2. Building height above 15 feet and second story additions to existing buildings must be set back a minimum of five feet from the front façade to preserve the scale of the original building.

SU-2/RTHD

The SU-2/RTHD Townhouse Residential Historic District zone corresponds to the RT Zone in the Zoning Code with the following exceptions:

A. Conditional Uses

1. A carport within the front yard setback is not allowed.
4. Walls, fences and retaining walls greater than three feet in height are not allowed in the front yard setback.
5. Existing non-conforming uses are to be treated as approved conditional uses.

B. Height

1. Buildings shall comply with the Building Height Limitations to Preserve Solar Access, §14-16-3-3 (A) (7) of the Zoning Code. The additional height limitations of this section shall apply to any lot regardless of the age of the building, except that structures existing as of the date of adoption of the Plan are allowed subject to Zoning Code regulations for buildings that are nonconforming as to height.
2. Building height above 15 feet and second story additions to existing buildings must be set back a minimum of five feet from the front façade to preserve the scale of the original building.

SU-2/MRHD

The SU-2/MRHD Mixed Residential Historic District zone corresponds to the RT Zone in the Zoning Code with the following exceptions:

A. Conditional Uses

1. Uses listed as permissive in the R-2 Zone are conditional uses in this zone.
2. A carport within the front yard setback is not allowed.
3. Walls, fences and retaining walls greater than three feet in height are not allowed in the front yard setback.
4. Existing non-conforming uses are to be treated as approved conditional uses.

B. Height

1. Building height shall not exceed 26 feet. Buildings shall comply with the Building Height Limitations to Preserve Solar Access, §14-16-3-3 (A) (7) of the Zoning Code. The additional height limitations of this section shall apply to any lot regardless of the age of the building, except that structures existing as of the date of adoption of the Plan are allowed subject to Zoning Code regulations for buildings that are nonconforming as to height.

2. Building height above 15 feet and second story additions to existing buildings must be set back a minimum of five feet from the front façade to preserve the scale of the original building.

C. Parking

No more than two parking spaces shall be allowed in the 15 foot front yard setback of any premises, in addition to a driveway. The remaining front yard setback shall be landscaped.



R-1 – West of Washington Street

A. Solar Access

1. Properties west of Washington Street zoned R-1 shall comply with the Building Height Limitations to Preserve Solar Access, Sec. 14-16-3-3(A) (7) of the Zoning Code. The additional height limitations of this section shall apply to any lot regardless of the age of the building, except that structures existing as of the date of adoption of the Plan are allowed subject to Zoning Code regulations for buildings that are nonconforming as to height.

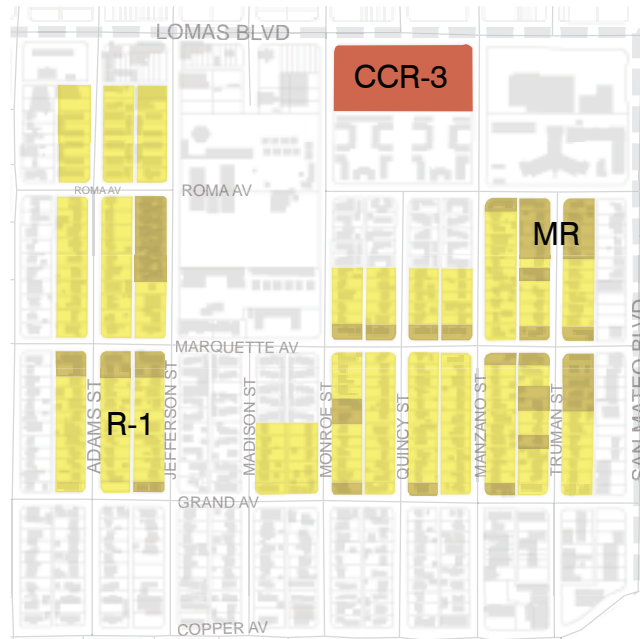
Highland Neighborhood

Issues and Analysis

In the late 1950's and through the 1960's the concept of "urban centers" along major arterial corridors was very prevalent in the southwest. Phoenix, in particular, developed a plan to establish a number of these centers along their Central Avenue west of downtown. Albuquerque in an apparent attempt to model this concept established the area around Central and San Mateo Avenues as this community's first urban center. This idea coincided with the establishment of zoning in Albuquerque in 1959 and lead to the establishment of high intensity yet segregated land use zoning in the Central and San Mateo area. In the Highland Neighborhood this took the form of R-3 zoning for the interior of the neighborhood to provide the opportunity for high density housing to support this coming center of activity. Apparently this R-3 zoning was put into place even though there were many single family housing units already built in the area. Today the majority of non-commercial areas in the neighborhood have been built as single family dwelling and the existence of the R-3 zoning is an ever present destabilizing influence in the neighborhood.

In an attempt to stabilize the zoning and future land use of the Highland Neighborhood this Sector Plan proposes to accomplish changes to protect single family blocks. The R-3 properties that are currently developed as single family to R-1 are rezoned from R-3 to R-1. Existing duplexes, townhouses, or homes with second units within these blocks are rezoned from R-3 to MR. Parcels and blocks developed with apartment buildings or predominantly higher density multi-family, remain R-3.

The properties that are proposed for rezoning are shown on the enlarged segment of the Zoning Map.



Zoning Proposals

In the Highland area between Copper and Lomas from Washington to San Mateo, the plan designates single family homes as R-1. Properties that have existing apartments, duplexes, and accessory dwelling units are rezoned as SU-2/MR, a zoning category existing in the 1987 Nob Hill Sector Development Plan. The intent of these changes is to support the overall health of the area, and ensure stable land use for homeowners. The zoning maintains the mixed densities in the neighborhood, but discourages the deterioration of existing single family homes.

Zone Change

Areas rezoned from R-3 to R-1 and MR are shown in the zoning map.

The R-1 Single Family residential zone is the same as the R-1 zone in the Zoning Code.

The MR Mixed Residential zone corresponds to the RT Zone in the Zoning Code with the following exceptions:

- A. Conditional Uses:
 1. Uses listed as permissive in the R-2 Zone.
 2. Existing non-conforming uses are to be treated as approved conditional uses.
- B. No more than two parking spaces shall be allowed in the 15-foot front yard setback of any premises, in addition to a driveway. The remaining front yard setback shall be landscaped.

VI. APPENDICES

APPENDIX A. A VISION FOR THE FUTURE OF THE HILAND THEATRE AND HIGHLAND NEIGHBORHOOD

**Presented to the City of Albuquerque for Inclusion in
The Nob Hill / Highland Sector Development Plan
February 2006
Our Commitment to the People We Serve**

Bernalillo County owns the Hiland Theater and the block of property on which it is located. We are concerned with the future of the Highland area both as a property owner and because of our commitment to serving the public interest. Our vision for the future of the Hiland Theater and the Highland Neighborhood is based on this commitment. A vision for the future of this area of our county should offer the most potential for the present and future neighbors, and offer a desirable destination for all of the residents in our county and our region. We believe this vision statement meets the test of public service today and in the future.

A Vision for the Future of the Hiland Theater and Highland Neighborhood

Our vision for the future may be summarized by the following description of the theater and neighborhood in the year 2030. The Hiland Theater is the hub for dramatic and performing arts serving Albuquerque and the region, with an increasing creative contribution to our State and Nation. The Hiland Theater is a destination for persons being trained in various art forms, and for persons attending music and theatrical performances. Transportation for the Hiland Theater and Highland

Neighborhood is provided by both transit and automobile. Transit, bicycle and pedestrian mobility are more common than use of personal passenger cars. When automobile congestion led to new definitions of the lowest level of service at the end of the first decade of the 21st Century, demand for transit grew and public and private providers responded. The Highland neighborhood benefited by having planned for and stimulated Transit-Oriented Development along with pedestrian and bicycle-friendly networks. We encourage action now to enable this vision to become reality by the year 2030. We have already begun. The Albuquerque Metropolitan Planning Area goals include increasing the utilization of transit, biking and walking. In implementing this goal, Central Avenue is designed now and for the future as a Premium Bus Route. (2025 Metropolitan Transportation Plan, 2003) Central Avenue is part of the Long Range High Capacity Transit System for the Albuquerque Metropolitan Planning Area. (Long Range High Capacity Transit System, 2002) Central Avenue is also designated as a bicycle corridor. (Long Range Bikeway System, 2004) The inclusion of Highland Transit-Oriented Development, pedestrian and bicycle-friendly networks in the Nob Hill / Highland Sector Development Plan is consistent with regional plans and is another important step forward in revitalization.

Realizing the Vision

Integrating Land Use and Transit

Integrating land use and transit is key to managing the impacts of rapid growth. With a more innovative approach to planning and development, we can curb unrestrained sprawl and invigorate our existing communities as better places to live and work. These plans identify hubs of activity, and encourage a higher intensity of residential and employment growth around transit stations. The objective is to provide a pleasant, lively and safe Highland community that offers a wide range of transportation choices, to alleviate the negative impacts of and limitations of primary access by personal passenger car. These are some of the community benefits of Transit Oriented Development. (Cervero, *et. al.*, 2004)

Transit Oriented Development

Transit Oriented Development encourages a mix of land-uses, such as residential, office, shopping, civic uses and entertainment within easy walking distance from a transit station. This is typically presented as a 1/4 mile, 5-minute walk. Map 1 shows a 1/4 mile radius around the Hiland Theater. The boundary extends from Washington Street on the west to San Mateo Boulevard on the east. Map 2 shows an initial, notional version of a five- and ten-minute walking contour based on current connections. A comprehensive walking contour map would identify pedestrian impedances and access improvements.

(Schlossberg, 2006) As neighborhood sidewalks are improved, for example, walking contour maps extend. Walking contour maps and related models can help us measure the pedestrian impact of Highland area improvements. We have the beginning of a pedestrian service performance measure for public accountability. The public rightly expects more than commitments from its government. It expects measurable improvements to realize government commitments. The mix of land uses associated with Transit Oriented Development, combined with thoughtfully designed community spaces, plazas, and similar features, forms a vibrant village-like neighborhood where people can live, work and play. Such a village is compact in size, pedestrian-friendly in design, can be customized to offer a wide variety of housing options, with convenient access to services, jobs, and a variety of ways to get around.

Some of the land-use actions to implement Transit-Oriented Development are as follows.

- Provide an exciting mix of housing, employment, entertainment, education, retail and services. Such complementary uses make an area more rich, interesting and convenient. They also generate human traffic and activity throughout the day, making an area safer, and increasing options for consumers and transit users.
- Discourage uses that are auto-dependant, such as drive through windows, car wash, and car sales lots.
- Encourage and give incentives for uses that generate pedestrian activity, particularly at the ground-floor level such as, shops, services, and offices.
- Attract special traffic generators, such as cultural and civic anchors that act as destinations. Examples are a library, theater, museum, recreation center. For the Hiland

Theater area, we propose a consistent emphasis on the arts.

- Create both vertical and horizontal mixed-use. A combination of uses on the same site makes for an interesting and more functional village.
- Encourage a mixture of housing types - for sale, for rent, market rate, affordable, senior housing.
- Preserve and protect the existing, stable neighborhood.
- Enhance existing land uses to make them more transit and pedestrian friendly.
- Locate the transit station in the heart of the development so it both generates and benefits from activity. Our vision is that the Hiland Theater would provide this hub.
- Concentrate the highest level of density closest to the transit station, gradually decreasing as development moves away from the core, creating the sense of a town center.
- Commercial development should be most intense within the core, creating an economic center. Generally, minimum densities for new residential development within a mile from station should be 25-30 (net) dwelling units per acre or greater. Between and mile, development becomes less compact and should allow for 15 dwelling units or greater.
- Mixed-use buildings and non-residential buildings should target a floor-area ratio of 0.75 (net) minimum within the mile from station, and 0.50 (net) floor-area ratio between and mile. This reduction in building set backs provides more space for homes and businesses, while framing the streetscape to make a more pleasant pedestrian experience.

Economic Revitalization

Transit-Oriented Development has the potential to revitalize the economy of the Highland area. National studies show that Transit Oriented Development increases land values. (Porter, 1997)

Properties within a Transit-Oriented Development can experience a substantial premium – 25% or more is not uncommon - over similar properties outside of a Transit Oriented Development. (Cervero, *et. al.*, 2004) Retail and office buildings experience lower vacancy rates and increase in rental rates. A portion of the increased premium is due to the comparative density, but a portion is also due to the desirability of these areas, and the amenity value of transit. Urban developers appreciate the value of transit. (Dunphy and Porter, 2006) Transit-Oriented Development creates a “critical mass” of activity that benefits surrounding businesses and generates significant economic activity for the community beyond the boundaries of the transit village itself. Transit-Oriented Development can result in economic revitalization of the Hiland Theater and Highland Neighborhood. Support for success includes the following.

- Transit-Oriented Developments, like all mixed-use projects, need market, infrastructure and zoning to succeed. *Transit alone is not enough!*
- Transit-Oriented Developments are often more expensive to entitle, design, construct, finance and manage. They require more front-end equity, and often take longer to bring to stabilization. In the long term, however, they also return the highest yields to the developer.
- Transit-Oriented Developments are relatively new, can be complex, and therefore benefit from experienced designers and developers.
- Most Transit-Oriented Developments require financial participation in some form from both the public and the private sectors. Public participation in the form of infrastructure construction, tax exempt financing and/or tax increment financing is not unusual.
- Partnership in Transit Oriented Development, such as that between Bernalillo County and the City of Albuquerque in the Highland Neighborhood, is a foundational principle



for success. (Dunphy and Porter, 2006) Each Transit-Oriented Development project is unique in that it must be designed for a particular community and location. It requires a partnership between the public and private sectors and often requires a rezoning of the area. The requirements for excellent design, high quality materials, open space, plazas and public amenities drive up the private investment required. Financing can be complex, as is finding developers with experience in mixed-use villages. As a result, it is not unusual for a project to take four to seven years from conception to completion. But the long term success of these projects is nonetheless attracting more and more attention from the development community, creating more examples and unique communities. (Cervero, *et. al.*, 2004)

Recommendations

Bernalillo County is committed to partnership with the City of Albuquerque and the Highland Neighborhood. We want to make sure that this vision for the future is shared, because to realize the vision will require that we work together. We offer the following recommendations for consideration in the Nob Hill / Highland Sector Development Plan.

We recommend the Highland Transit-Oriented Development be included in the Sector Development Plan, with the Hiland Theater as the hub.

We propose discussion of a Joint Powers Agreement between the City of Albuquerque and Bernalillo County. The purpose of the discussion would be to draft an agreement on support of Highland Transit-Oriented Development.

With support of the Sector Development Plan, and agreement with the City of Albuquerque, re-zoning

to higher densities would be appropriate. We would also identify a tax increment financing plan for consideration by the Bernalillo County Commission and the City of Albuquerque. With this coordination and cooperation, we can then issue a Request for Proposals to select a private sector development partner.

It will require patience and resolve to realize Highland Transit Oriented Development. It will call for our best effort in planning and programming. It will require leadership in our public agencies and in the community. It is, we believe, a compelling vision that is worth the best we have to offer the people we serve.

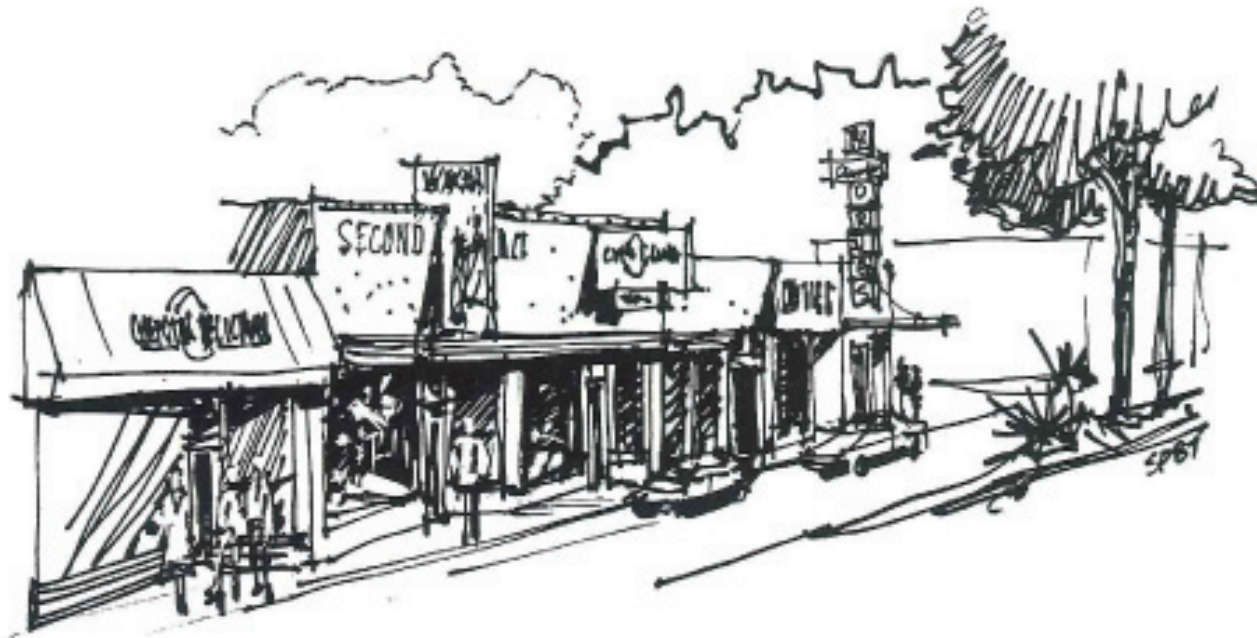
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- _____, *Long Range Bikeway System*, Albuquerque Urban Area, Metropolitan Transportation Board of the Mid-Region Council of Governments, February 12, 2004.
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Appendix B. Excerpts from Revitalization through Design Guidelines for Nob Hill Business District





Whether beginning with an existing building or a vacant lot, you will want to consider some basic tenets in planning your course of action. As a merchant, professional, property owner or property manager, you recognize that your building communicates a lot to customers, clients, and neighbors.

Storefront and building improvements and new construction projects which are based on sound design principles can bring new life to the commercial district and contribute to the success of every business in the Nob Hill area. Changes and additions which are not sensitive to the best qualities of Nob Hill are more likely to create a disharmony of elements and negative appearance.

Improving Existing Buildings



Reasons for improving your building or facade include:

- **Increasing your business.**
A facelift will improve the image of your business, attracting new customers and letting old customers know that you are an active, ongoing concern.
- **Attracting tenants.**
If you are a landlord, you will find a renewed appearance will attract quality tenants.
- **Increasing the value of your property.**
Your property or leasehold will have a greater value in the marketplace if its appearance is equal to or better than that of its neighbors.
- **Extending your building's useful life.**
Building improvements and maintenance will greatly add to the number of years that you can expect to have a return on the investment you have made in your buildings.

The types of improvements you can make to your buildings or facade generally fall into one of the three following categories:

REHABILITATION means taking corrective measures which improve the appearance of a building while preserving the essential historical or architectural character of the structure. Of necessity, rehabilitation may require the introduction of entirely new elements in order to adapt to changing needs and modern materials; but this is balanced by care to preserve valuable existing qualities.

In storefront row buildings built as a single unit, the integrity of the row can best be preserved and enhanced through uniform treatment of the individual facades with respect to such elements as color, cornice line and decorative materials. Storefronts built as individual units may allow for more liberal interpretation during rehabilitation, as original development included greater variety of treatment.

Roadside architecture or drive-in/setback buildings are varied in configuration, use and style, calling for careful consideration in the planning of rehabilitation.

In stand-alone buildings, the key issue is one of strengthening the relationship of the building with the street and surrounding buildings while inviting pedestrian access. Utilization of such devices as landscaping, low walls and arcades at the property line will reinforce the connection of the stand-alone structure with the street. Primary entries and windows — preferably display type — should be kept in the street facade, and pedestrian access should be clearly delineated and accented with lighting.

The primary goal in rehabilitation is to **return the building to good working order.**

RESTORATION means the repair and renovation of a building with the intent to return it to its original appearance at some specified time. Historical research has shown that many of the buildings in the Nob Hill area originally had interesting design features which have been covered over or obliterated during the passing years.

The primary goal in restoration is to **return the building to its original condition.**

REMODELING is the alteration of a building sometimes to the extent that its essential and characteristic features are obliterated. While this form of "improvement" may be appropriate for some buildings in the Nob Hill area, many buildings would benefit the most from a rehabilitation which preserves their original character.

The primary goal in remodeling is to **provide a different look, or to accommodate new functions.**

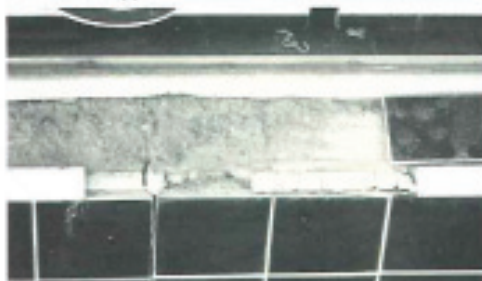
Getting started

The process of rehabilitating your building or facade can be an exciting and personally rewarding experience if you become an active participant in the design process.

The first step is to take a careful look at the exterior facade.

Look closely

Is the paint on the trim weathered or peeling? Is the stucco stained or cracked? Are there broken or missing tiles? How do the window displays appear to your customers? If you were shopping, would you be attracted by your building's appearance?



Look from across the street

How does your building fit in with the other buildings on your block? Examine the building's proportions, the size of the windows and entry, the rhythm of the block and materials and textures. Do the buildings on your block work together or compete with one another?



Research your building

What did it look like originally? How has it been altered over the years? Start by finding the date your building was constructed; check your ownership deed or ask your landlord. A photograph of its original appearance may be available in *Abuquerque Progress*, a magazine published between 1934 and 1964, available at the Albuquerque Public Library. Many of the buildings in the Nob Hill area built in the 1930s, 1940s and 1950s had interesting details and design features which have been lost to insensitive remodeling.

Consult with an architect

If your renovation or remodeling job will cost \$80,000.00 or more, your construction documents must be prepared by a licensed architect. Your architect will not only know how to make your renovations conform to the building and life safety codes, including the requirements for accessibility to handicapped persons, she or he will be able to help you develop a long-range design plan. With a long-range plan, you can accomplish your improvements in phases while you assure that all the work will create a unified appearance when it is complete.



3025 Central NE, 1940



1952



1955

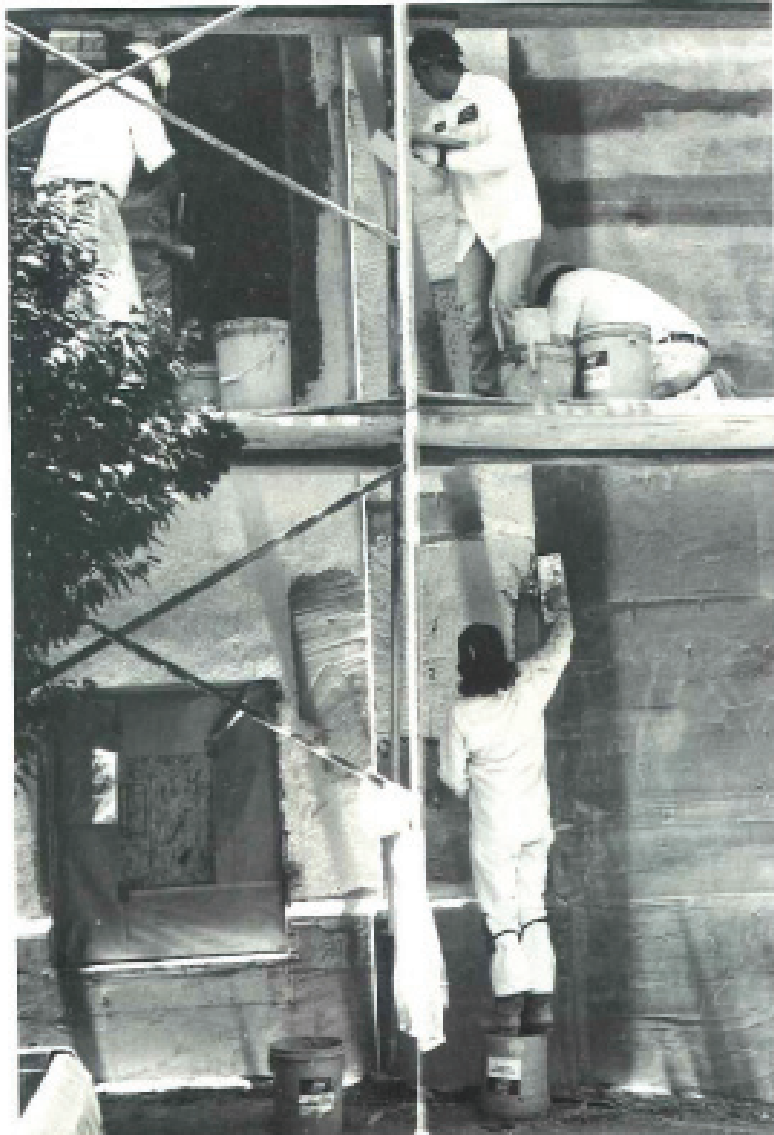
Follow these guidelines

If done properly, even small improvements such as painting, cleaning, new signage or awnings can have a major impact on the appearance and value of your building.

DESIGN RECOMMENDATIONS



New Construction



The scale and cohesiveness of the Nob Hill district are among its strongest features. New buildings or additions should be designed to fit harmoniously into the existing architectural context. For both small, individual infill projects and larger projects, new construction can enhance and strengthen the special character of the Nob Hill area by adhering to the design recommendations illustrated in these guidelines.

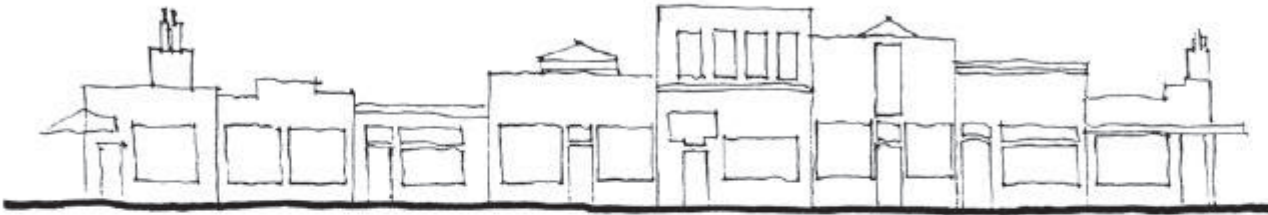
Unfortunately, recent construction trends have destroyed some of the Nob Hill district's special identity by breaking its scale and continuity. These trends include replacing storefront row buildings with setback buildings and siting parking lots directly on Central Avenue. If these trends continue, the Nob Hill district will become just another commercial strip undistinguished from other such strips in Albuquerque.

Before beginning new construction, get to know the district. Use an architect who is familiar with the area and who understands the importance of making new development fit.

New buildings may take many forms; they need not be historic replicas. More important is an awareness of how the pieces of new construction fit together with the other elements in the area. Because every site has its own design constraints and opportunities, prescriptions are not offered in these guidelines. If the designers of new buildings examine the qualities of the existing neighborhood to learn what principles create its special character, they can enhance and preserve the special environment of this segment of America's longest Main Street.

PAINr AND COLOR

Painting can be one of the most dramatic and least expensive improvements to a building. Painting at regular intervals, over time, is an essential part of maintenance and upkeep. It is also a practical way to visually tie together individual facades.

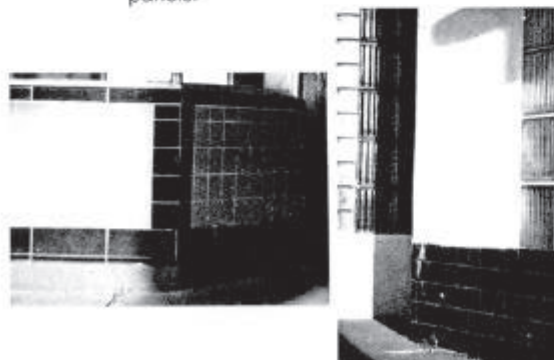
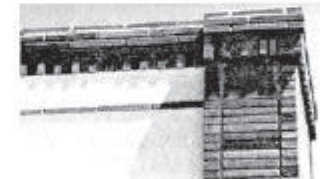


- ☐ Attention should be given to selection of appropriate colors, preparation of the surfaces, choices of paint type (oil or latex base) and finish (gloss, semi-gloss or matte). Normally, the previous paint type should be used in repainting. If possible, use oil base paint for wood.
- ☐ Color palettes and paint schemes on adjoining buildings should be compatible but not necessarily the same. A good rule of thumb is to use the same color to tie the storefronts together as originally designed where adjacent storefronts were built together as part of a row or large building. On the other hand, if adjacent storefronts were developed individually, they should read as separate units with different but compatible color schemes.
- ☐ Color should be used to unite building elements into a single composition, while at the same time highlighting important features like historic detailing, interesting motifs, special cornice treatment, signs, and storefront framing members.
- ☐ Color palettes should generally be lighter on the south side of the street, which is normally in shadow, than on the north, which is normally in the sun.
- ☐ Very dark and very bright colors are incompatible with most existing buildings and should be avoided unless there is a historic precedent.
- ☐ Some materials, like brick, ceramic tile, terra-cotta and glass should **never** be painted. Special techniques may be employed to maintain, clean, or repair and restore these materials.

MATERIALS

Use of appropriate building materials is a key to compatible rehabilitation. Materials common to the building styles in the Nob Hill district include: stucco, brick, porcelain enamel, terra cotta and structural glass. Ceramic tile is commonly used in the bulkhead below windows and as window trim.

Inappropriate materials include exposed adobe, slump block, wood siding, aluminum siding, shingles or shakes, and plastic panels.



- ☐ Wherever possible the original appearance as determined by on-site inspection, photographic or other documentary evidence, should be restored.
- ☐ New materials should be matched with older materials which are visible on the structure or which are compatible with other buildings on the block.

STYLES

The Nob Hill area is distinguished by its own particular variety of architectural styles. These contribute to the visual and historic interest of the district.

Because this area developed in the thirties, forties, and fifties, as a district oriented to the automobile, its general character is one of modernity. To be modern during these decades meant to abandon traditional styles for the sleek, machined look characteristic of International, Art Deco, and Streamlined architecture. At the same time, the strong local sentiment for Spanish-Pueblo and Territorial styles led to the incorporation of these stylistic elements into many buildings (see Appendix for a description of these styles and buildings of architectural and historic significance.).

- ☐ Learn about the architectural style, or style of ornament, of your building so you can repair or restore it to an appearance which will enhance its essential interest and appeal and remain in harmony with neighboring buildings.
- ☐ Changing an existing facade for a new one in a traditional period style such as Victorian or Colonial, or in the rough textured wood or adobe styles which expressed the return to nature of the sixties and seventies is inappropriate.
- ☐ Pseudo-historic styles detract from the character of the area by compromising what is truly historic.
- ☐ New buildings should offer contemporary interpretations of predominant styles in the district.

ROADSIDE ARCHITECTURE

The term roadside architecture is used to refer to buildings, beginning in the 1920s, which were located on the highway and oriented to the growing use of the automobile. These included gas and service stations, autocourts, cafes, and their modern day motel and fast food descendants. The designation of Central Avenue as Route 66 in the 1930s led to the early development of classic roadside architecture in the Nob Hill area. Many of the remaining thirties and forties examples of these building types represent outstanding architectural styles of their period. These buildings require careful consideration in their rehabilitation, whether for continuing the present use or for adaptive reuse. Some are worthy of restoration.

MOTELS

Motel is a term coined from the integration of motor hotel. Its predecessor, the tourist court or "campground," was generally made up of individual units or cabins. The "modern" motel which was developed during the late 1930s had an "L", "U", or "II" configuration. The older "II" shaped motel generally had a central office block. While the earlier motels were one-story linear

buildings set back from the highway, those of more recent vintage are generally larger and two-story.

The older motels in the Nob Hill area exhibit regional architectural styles including Pueblo Revival and Spanish Territorial Revival. These styles are important to our regional architectural heritage and the character of the Nob Hill area.



3701 Central SE, date unknown



4300 Central SE, ca. 1928

- ☐ The improvement or adaptive reuse of older motels exhibiting strong regional style should restore or enhance original architectural features and details.



- ☐ Additions to motels, where that is practical, should follow the traditional "U", "L", and "II" configurations. The central office building should be retained where that is an original feature.

- ☐ Infill of the parking area is not appropriate and may not be possible, given parking requirements.

- ☐ Vehicular access from curb-cuts should be clearly delineated and kept to a 24-foot maximum.

- ☐ In those cases where the central parking area fronts on Central Avenue, the connection between the street and the building should be defined through landscaping and low walls. This will also help to discourage loitering by unauthorized persons.

- ☐ Walls should match original building materials and colors.



4020 Central SE, 1958



1987

GAS AND SERVICE STATIONS

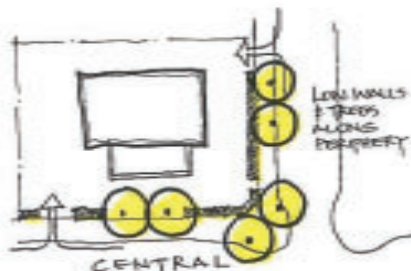
Yesterday, as today, gas stations were generally located on corners for both physical and visual access. Some of the best local examples of thirties and forties international style architecture can be found in the Nob Hill area's older gas stations. This style typically included rounded corners and cubist towers which allowed instant identification from a rapidly approaching automobile. Surviving structures in the Nob Hill area generally no longer fulfill their original use and many are candidates for rehabilitation and adaptive reuse.



3523 Central NE, 1997

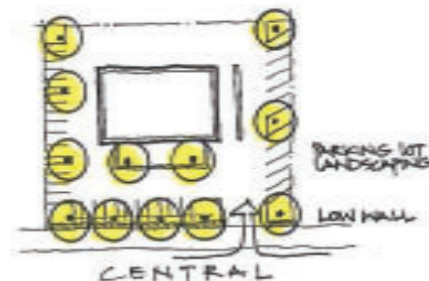
- ☐ The original form and character (proportions, massing, materials, and entry, door and window configuration) should be retained or restored.
- ☐ Any modern additions should be in keeping with the original character of the building.

- ☐ The connection with the pedestrian and the street should be strengthened through landscaping and low walls.
- ☐ Canopies, typical of period gas and service stations, should be retained and rehabilitated or restored.



DRIVE-IN RESTAURANTS

While there are no surviving examples of intact vintage drive-in restaurants in the district, their descendants — the fast food restaurant with a drive-up window — are found in the district. These newer buildings follow the old pattern of a small central building located in the middle of a parking lot.



3325 Central SE, 1997

- ☐ These set-back buildings should strengthen their connection with the pedestrian through parking lots landscaped to shade cars and provide visual relief from the expanse of asphalt.
- ☐ Landscaping and low walls should be located around the parking lot's perimeter.
- ☐ Curb-cuts should be limited to a 24-foot maximum and be clearly delineated.



Central Avenue Drive-in, near UNM, ca. 1943

BUILDINGS OF ARCHITECTURAL OR HISTORIC SIGNIFICANCE OR INTEREST

If your building retains some of its original character, or if old photographs or the removal of layers of material added later can reveal its original design, rehabilitation or restoration is probably the best way to enhance both the building and the block.

While some of the buildings in the Nob Hill district are of important architectural or historic merit and deserve the investment of money and effort for preservation and restoration, many more require careful consideration so that their best features are enhanced and not lost to hasty remodeling.

Unobtrusive vernacular building styles and small details, such as the brickwork and coping on the buildings at 3110 through 3120 Central SE, the terrazo tiles on the building at 3418 Central SE or the terra cotta facade on the buildings at 3412 and 2414 Central SE, contribute to the character of the Nob Hill district. Every building, no matter how modest, has a history and a story to tell.

This list of buildings of architectural or historic significance or interest is not exhaustive. Many buildings have been modified to the extent that their original architectural features are no longer apparent. Research and the removal of newer materials may reveal previously unsuspected architectural gems.

1930, 3211 Monte Vista Blvd. NE

Designed by T. Charles Gastra, the Monte Vista School is an exceptionally fine example of the Mediterranean style applied to institutional use. As well as architectural importance, it has local historic significance as the focal point of the Monte Vista Addition, the first subdivision east of the University of New Mexico and north of Central Avenue. The school is on the National Register of Historic Places.

1933, 3821 Central Avenue NE

Built in the pueblo revival style, the Aztec Motel (originally the Aztec Autocourt) is a good example of the "Oasis in the Desert" Route 66 Motel. With two room blocks facing in on a shaded central court and office, it offers both privacy and access to Route 66. (Illustration, page 32)

1936, 3201 Central Avenue NE



(NE corner at Bryn Mawr) Fire Station No. 3, renovated in 1984 as the Monte Vista Fire Station restaurant, was designed by E. H. Blumenthal and built by the WPA. It is a fine example of Pueblo Revival Style, achieving interest by its composition of massive

elements and ornamented with correct details of timber lintels, vigas showing its true framing, and picturesque ladders based on genuine Pueblo models. It is the only fire station in the United States built in this style. It is on the National Register of Historic Places. (Photo after renovation, see page 5)

1937, 3712 Central Avenue SE

Originally the Modern Autocourt, the Nob Hill Motel is one of the older surviving motels in the district. Its flat stucco walls and pronounced brick cornice are suggestive of the regional Territorial Revival Style. (See page 23)

1937, 3100 Central Avenue SE



(SE corner at Richmond) This is one of the first buildings in the district, constructed at the end of the Great Depression. Built in the Moderne Style, with a curved corner and black tile on the bulkhead, it has been in use as a pharmacy since its construction fifty years ago. It was originally built with a flat wall on the east, allowing for the subsequent construction of storefront row buildings. 3102 through 3108 Central SE were added during the 1940s but maintained the scale and decorative elements of the original pharmacy. Two of these additional buildings have been remodeled.

1938, 3013-3025 Central Avenue NE



The Lobo Theater was hailed as Albuquerque's "first deluxe suburban moving picture house." The theater, with its entrance on the street, extends behind the row of storefront shops built as part of the same project. This was the first major storefront row building in the district. Originally a striking Moderne Style building accented with sleek red and cream colored tiles on the bulkhead, its shopfronts, over the years, have been remodeled in a variety of ways. The theater has been in continuous use since 1938. (See photo, ca. 1938, page 4)

1939, 3226 Central Avenue SE

(SW corner at Wellesley) When the Jones Motor Company, the first of the automobile dealers to do so, moved from downtown out to suburban Route 66, the young designer Tom Danahy created an "extremely modernistic" building in early International Style to house the automobile showrooms and associated services, with an elegant sculptural tower easily seen by motorists traveling in either direction. This is probably the most important remaining building by this talented architect, who died in an accident in the

forties. Its elegant simplicity and attractive composition of spacious volumes, with the drama of the semicircular automobile showroom at the front, express glamour and luxury. (See page 6)

1939, 3601 Central Avenue NE



(NE corner at Carlisle) Originally housing the Nob Hill Service Station, this is a classic example of a Moderne roadside building with curved volumes and a streamlined cornice. Over the years the building has undergone some remodeling. The east and west garage doors have been filled in, and the canopy was removed.

1939, 4300 Central Avenue NE



(NW corner at Washington) The DeAnza Motel is executed in a modified Pueblo Revival Style. Built on a grand scale, it has murals depicting scenes from New Mexico's past.

1946, 3523 Central Avenue NE



(NW corner at Carlisle)
This is the last remaining Highway 66 gas station of its type, and it is a particularly beautiful example. It was probably designed by Tom Danahy, and like his building for Jones Motor Company is in the early International Style, with a tower — to catch the eye of the motorist — which is like a constructivist sculpture. Against a background of trees with the mountains beyond, it is the sculptural centerpiece in the view from the restaurants of the Nob Hill Business Center across the street.

1946, 3001 Monte Vista NE



(NW corner at Dartmouth)
Designed by T. Charles Gastra, the Hendren Building is a sparkling example of the International Style, finished with Art Deco materials: structural glass, pink marble, and polished aluminum. It is no larger than many other buildings of the time and place, but is unique

in its richness and the strong period feeling created with just a few materials, exquisitely composed.

1946, 3509-3513 Central Avenue NE



This modern commercial row was designed by A. W. Boehning, Sr. It contains interesting and well-proportioned details including curved pilasters built into the facade and a ribbed concrete facade accented with green and pink tile. The eastern-most storefront was covered using black ceramic tile in 1957.

1947, 3500 Central Avenue SE



(SW corner at Carlisle)
The Nob Hill Business Center is of historical significance as Albuquerque's first drive-in shopping center. The "U" shaped center was originally designed in the Southwestern Territorial Revival Style. Its design was modified prior to construction, creating a marriage of Moderne and Southwestern Styles. Its

curved white stuccoed walls are accented with a brick coping at the cornice line, and string courses of brick band the center to provide horizontal definition. Details include: Deco-inspired towers illuminated with colored lights and terra cotta tiles used at the bulkhead level and above some storefronts. Designed originally by Louis Hesselden, it underwent a sensitive renovation by Van Gilbert in 1984.

1949, 105-109 Amherst SE



This commercial row, designed by Max Flatow, is an exceptional variation on the Central Avenue commercial vernacular style. The parapet of each bay rises separately above the roofline in an exaggerated interpretation of a Central storefront roofline. Blue green terra-cotta tile over the window fronts adds a Deco touch to the facade and provides a delicate contrast to the building's pink toned stucco. Eyelet awnings add depth to the flat facade.

1949, 3205 Central Avenue NE
Built during the late 1930s, the American Legion Building (now Thunderbird Travel) underwent extensive remodeling in 1949 with the



addition of a second story and a new facade. These additions, designed by A. W. Boehning, Sr., are in a Moderne Style with modest Art Deco features.

1958, 3022 Central Avenue SE

SW Corner at Richmond
The First Interstate Bank, designed by W. C. Kruger & Associates and built as the Bank of New Mexico, is an example of the late International Style. Its magnitude alone gives it an importance in the district; it is also a fine example of its period. It has the vigorous simplicity which became the ethic of this style, and is enriched by its finish of stone panels and polished aluminum.



ARCHITECTURAL STYLES

International Style

The International Style was introduced in the United States by an Exhibit at New York's Museum of Modern Art in 1932, accompanied by the publication of the book *The International Style* by Henry-Russell Hitchcock and Philip Johnson. In 1939, the talented young Albuquerque Architect Tom Danahy used this supremely up-to-date style for the Jones Motor Company building at 3226 Central NE (see "Buildings of Architectural or Historic Significance"). Danahy probably also designed the gas station on the northwest corner of Central and Carlisle. These buildings do not have traditional ornaments but are characterized by pure, smooth surfaces which express the volume of the space enclosed in the building. Materials which can be used to create large uniform surfaces are characteristic of the International Style: stucco and large unified areas of glass, in these examples.

Art Deco

Art Deco is a style named for another important exhibition, the 1925 Exposition of Decorative & Industrial Art (Exposition des Arts Decoratifs et Industriels) in Paris. True to this name, it is primarily a style of ornament: modern ornament made of modern materials. It is characterized by rich colors; smooth, highly finished surfaces; and abstract design. In Nob Hill, glossy ornamental bands or panels of structural glass or ceramic tile, and shining storefronts of glass and aluminum are the most typical Art Deco elements to be found.

In Albuquerque, the blending of Art Deco with the geometric ornament of Native American designs, and of

the abstracted International Style with the geometric forms of Pueblo and Hispanic adobe architecture, created the style called Pueblo Deco. The best examples of this style are found on Central Avenue in downtown Albuquerque. These include: the Kimo Theatre (1927), 423 Central NW; the Maisel Building (1937), 510 Central SW; the Skinner Building (1931), southwest corner of Eighth and Central; Wright's Trading Post, 616 Central SW.

Streamlined Moderne

Streamlined Moderne is an American variation of the International Style, which rejected external decoration on buildings in favor of unbroken surfaces.

Streamlining expresses modernity through emulating the high-tech machines of the thirties, locomotives and DC-3s streamlined to reduce their air resistance. On buildings, the locomotive's teardrop shape was usually impractical; but rounded corners, bands of polished aluminum sweeping around these corners and along the tops of windows, and even incised lines in the building surface — like the lines in a cartoon drawing which express the motion of characters and objects — produce a style often called Moderne or Streamlined Moderne. This style was most popular from 1930 to 1950.

Spanish-Pueblo Revival

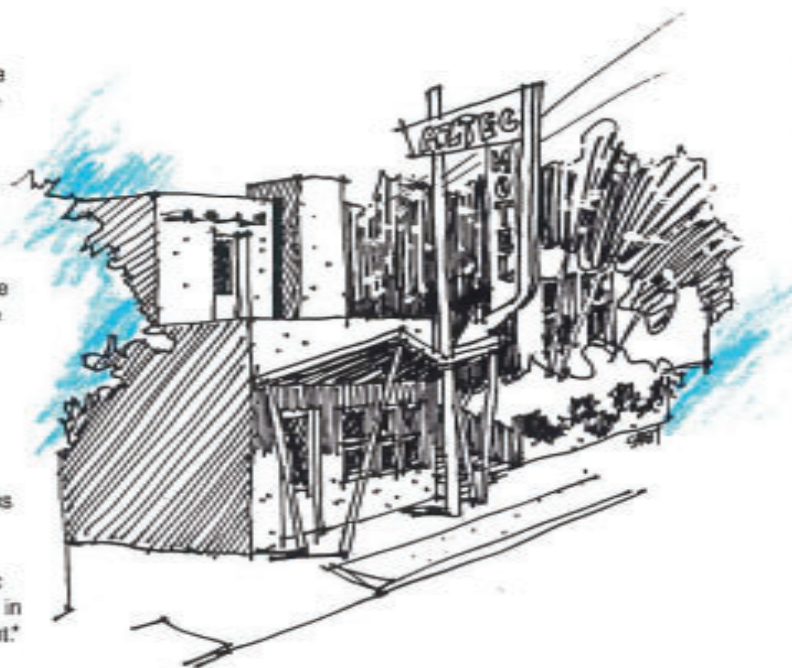
This style is most important in Albuquerque, not only for its widespread use, but also because the first Pueblo Revival buildings were built here on the University of New Mexico campus under the direction of President William George Tight in the years between 1906 and 1910. Modeled on the

Pueblo villages of the Southwest and on the Spanish Mission churches built in these Pueblos, the style either uses adobe or imitates it by means of a stucco finish. The distinctive battered walls and rounded parapets give a sense of mass and solidity. Roofs are flat; early versions have projecting vigas (roof beams) and canales (rain gutters). Exposed wood lintels above windows are common, as are flat-roofed portales supported by round wood columns topped by corbels.

Pueblo style buildings constructed after the Second World War achieve their image largely through the use of battered walls, adobe colored stucco and recessed windows.*

Territorial Revival

The Territorial Revival style is a natural outgrowth of the Spanish Pueblo style and is modeled on the style of adobe building fashionable during the early years of Anglo-American occupation of New Mexico. Buildings constructed of adobe or stuccoed to resemble it; brick copings, pedimented lintels and wooden dentil courses over large windows and doors are marks of this style. The Territorial Revival style is most frequently used for large houses, churches and public buildings. The style has continued in popularity from 1925 to the present.*



*Adapted from *The Old House Workbook*, Neighborhood Housing Services of Albuquerque, 1960.

GLOSSARY

Definitions are from Webster's New International Dictionary, Second Edition, unless noted otherwise. Note (Ed.) means by the editors. Note (Harris) means from Dictionary of Architecture and Construction, edited by Cyril M. Harris.

Art Deco See the section on styles.

Articulated Expressed in separate items or particulars; distinctly marked off; formulated in clearly distinguished parts. In building plan, having separate spaces pulled out and shown as separate volumes of a building; in facades, having elements such as cornices, pilasters, lintels, and window and door frames relieved, marked with moldings and other ornaments, and recessed.

Bulkhead The member of an entrance frame which forms a base for a sidelight adjacent to a door. (Harris, Dictionary of Architecture and Construction) By extension, the wall which forms a base under a storefront show window.

Carrara glass See Structural glass.

Constructivism Nonfigurative art of a school founded in Moscow in 1920 as a secession from Suprematism, concerned with formal organization of planes and expression of volumes and using modern industrial materials (as glass and plastic).

Coping The highest, or covering course of a wall, often with sloping top to carry off water. Copings are commonly cut with a drip.

Divided light A glazed opening divided into panes by muntins. If the muntins are secondary framing members, and each pane is a separate piece of glazing material, it is a genuine divided light. "Snap-in muntins" are plastic or wood frames which can be installed against a single light to give it the appearance (from one side) of being divided. (Ed.)

Drip A throat; a groove cut along the underside of a member (as a stringcourse or coping on a wall) to prevent water from running back across it (toward the wall). (Harris)

Entry That by which entrance is made; a passage; vestibule.

Facade The front of a building; also, a face, (as a flank or rear facing on a street or court) of a building that is given emphasis by special architectural treatment.

Fenestration The arrangement and proportioning of windows; hence, the decorating of an architectural composition by the window (and door) openings, their ornaments, and proportions.

Flared corner Sidewalk design at a street corner such that the sidewalk widens into the vehicular street. (From flare: to open or spread outward.) (Ed.)

Flat roof A nearly horizontal roof pitched for water drainage only. (A dead flat roof is one which does not drain.)

Glass block Technically, glass masonry units. Hollow or solid blocks of glass constructed so as to be capable of being laid up in mortar, with reinforcements, in a similar way to masonry of brick, stone, or concrete blocks. (Ed.)

Glaze To fit, set, or equip (as a window frame) with glass; to cover or protect with glass; to enclose, case, or wall with glass.

Light The medium through which light is admitted, as a window, or pane in a window, a skylight. (Webster) A pane of glass, a window, or a compartment of a window. (Harris) In the window and glass trades, often spelled "lite" when the meaning is a single pane of glass or a single compartment of a divided light. (Ed.)

Lot An allotment of land, as to a settler. Hence, a distinct portion or plot of land. One of the smaller portions of land (as a division of a block) into which cities, towns, or villages are laid out, the size varying with the locality. (Webster) The typical lot on Central Avenue in the Main Street area is 25 feet wide (street frontage) and 100 feet long (to the alley). (Ed.)

Moderne Stylishly modern in the modes of the period between the World Wars. See the section on styles. (Ed.)

Mullion A slender bar or pier forming a division between lights of windows, screens, etc. An upright member of a framing.

Muntin 1. A secondary framing member to hold panes within a window, window wall, or glazed door; also called a glazing bar, sash bar, window bar, or division bar. 2. An intermediate vertical member that divides the panels of a door. (Harris)

Opening A place or part which is open, an aperture. (Webster) Thus, in walls, openings include doors, windows, gates, and unenclosed apertures. (Ed.)

Parapet A low wall, or similar barrier, as a railing, esp. one to protect the edge of a platform, roof, bridge, or the like. (Webster) Typically, in street front construction, the extended top of the facade which hides the roof and rooftop equipment from view. (Ed.)

Rehabilitation Putting onto a proper basis or into a previous good state: restoration (as of something damaged or decayed) to a state of efficiency and good management; improvement to a higher level or greater value. (Webster) For buildings designated as historic, the owner will want to adhere to the Secretary of the Interior's "Standards for Rehabilitation and Guidelines for Rehabilitating Historic Buildings," which can be obtained from the New Mexico Historic Preservation office in Santa Fe. (Ed.)

Remodeling Alterations within the existing fabric of a building, as distinct from additions. (Harris)

Renovate To restore to life, vigor, or activity: revive, regenerate; to restore to a former state (as of freshness, soundness, purity, or newness of appearance); make over, renew.

Restoration Bringing back or putting back into a former position or condition; the process of putting a building back into nearly or quite the original form; also, the making of drawings or models or both designed to show the conceived original form of a building (as a ruin).

Sidewalk signs Signs oriented to the pedestrian on the sidewalk rather than to the passerby in a vehicle. (Ed.)

Streetwall The composition of the facades along a commercial block at the building property lines, which creates a uniform wall enclosing the sidewalk and street on each side. It is this streetwall which clearly defines the street and establishes its character as an urban place.

Stile One of the upright pieces in framing or paneling; one of the primary members of a frame, into which secondary members are tenoned.

Strip A street or avenue densely lined on both sides by a large variety of retail stores, gas stations, restaurants, bars, etc. (Random House) "Strip" as it is commonly used refers to an area where businesses are primarily oriented to automobile rather than pedestrian traffic. (Ed.)

Structural glass A finish material of pigmented glass, manufactured in the form of tiles. The manufacturers called it "structural glass," and this name for the material was commonly accepted. Carrara glass was the name given to the product manufactured by Pittsburgh Plate Glass Company. Marietta Manufacturing Company produced Sani Onyx, and Libbey-Owens-Ford produced Vitrolite. (Ed.)

Transom 1. A horizontal crossbar in a window, over a door, or between a door and a window or fanlight above it, as distinguished from a mullion, or vertical bar. 2. A window above a door or other window, built on, and commonly hinged to, a transom. Also called a transom window.

APPENDICES

Appendix C. Funds Appropriated for Nob Hill

2/3/2006

	A	B	C	D	E	F	G	H
1	Funds Appropriated for Nob Hill							
2								
3								
4	Activity #	Source	Responsible Agency	Scope	Appropriated	Encumbered/ Expended	Balance	Notes
5								
6	7389730	01 GO / S-A	CIP/DMD	Design & construct streetscape improvements at the intersection of central Ave & Morningside Drive	\$300,000	\$37,694	\$262,306	Design fees incurred
7								
8	7389860	01GO / CS-A	DMD Streets	Design & Construct Street Lighting in the Nob Hill Area	\$275,000	\$89,816	\$185,184	Extensive amount of lighting installed or upgraded on Central and on Silver to the south and Copper to the north. Paid for out of other City funds. Area received the amenity and now has additional funds available.
9								
10	7100260	05 Clean Up	DMD Transportation Development	Funds allocated by Councilor for Central & Morningside to construct pedestrian amenities	\$250,000	\$0	\$250,000	Transportation development studying intersection bump outs. Funds could be used for this purpose
11								
12	7100260	05 Clean Up	DMD Transportation Development	Funds allocated by Councilor for Central & Wellesley to construct pedestrian amenities	\$250,000	\$0	\$250,000	Transportation development studying intersection bump outs. Funds could be used for this purpose
13								
14	7256050	04 SG	DMD Traffic Engineering	For Phase 1 of pedestrian lighting improvements to complement street lighting enhancements & intersection improvements R-061-059-2004	\$130,000	\$0	\$130,000	Significant O&M implications associated with pedestrian street lighting. Operating implications include retail cost of electricity. Maintenance implications include replacement of non-standard fixtures. Standard lighting in the area may be upgraded, but at increased cost of electricity.
15								
16	7256040	04 SG	DMD Traffic Engineering	To Purchase & install phase 1 of the Nob Hill Highland pedestrian lighting. R-061-059-2004. Oct 04 CU: R191-000-05: Lights are over sidewalk & are the luminaries are 12-15 feet high	\$75,000	\$0	\$75,000	Significant O&M implications associated with pedestrian street lighting. Operating implications include retail cost of electricity. Maintenance implications include replacement of non-standard fixtures. Standard lighting in the area may be upgraded, but at increased cost of electricity.
17								
18	7260220	04GO / CS-A	DMD Transportation Development	Traffic Signal Wellesley & Central	\$100,000			Not Warranted
19				Traffic Signal Morningside & Central	\$100,000			
20				Pedestrian Flashers @ Central				Insufficient funding
21				Eastbound, east of Girard				
22				Westbound, west of Washington	\$25,000			
23					\$225,000	\$0	\$225,000	
24				Sub-Total	\$1,505,000	\$127,510	\$1,377,490	
25								
26								



APPENDICES

2/3/2006

	A	B	C	D	E	F	G	H
1	Funds Appropriated for Nob Hill							
2								
3								
4	Activity #	Source	Responsible Agency	Scope	Appropriated	Encumbered/ Expended	Balance	Notes
27	2005 STATE GRANT FUNDS AGREEMENT FINALIZED IN NOVEMBER 2005							
28	7311100	05 SG	Planning	To plan for public improvements, including improvements to streets, sidewalks and transit stops in the Central Highland Upper Nob Hill Metropolitan Redevelopment Area	\$50,000	\$0	\$50,000	Funds unavailable prior to November 2005
29								
30	7311050	05 SG	Planning	For the Metropolitan Redevelopment Area project for public improvements, including improvements to streets, sidewalks and transit stops in the Nob Hill Area	\$75,000	\$0	\$75,000	Funds unavailable prior to November 2005
31								
32	7311010	05 SG	Planning	To design, construct, purchase & install pedestrian lighting in the Central Highland Upper Nob Hill Metropolitan Redevelopment Area	\$200,000	\$0	\$200,000	Funds unavailable prior to November 2005. Significant O&M implications associated with pedestrian street lighting. See lines 14 & 15 above.
33								
34	7305140	05 SG	Planning &/or Family	For exhibition & Office space renovations at the City-owned DeAnza Motel	\$10,000	\$0	\$10,000	Funds unavailable prior to November 2005
35								
36	7311060	05 SG	Planning	For the Metropolitan Redevelopment Area project for public improvements, including improvements to streets, sidewalks and transit stops in the Nob Hill Area	\$125,000	\$0	\$125,000	Funds unavailable prior to November 2005
37	Sub-Total				\$460,000	\$0	\$460,000	
38								
39	Total				\$1,965,000	\$127,510	\$1,837,490	
40								
41								
42								
43								
44								
45								



Appendix D. Prototypical Intersection Drawings and Pedestrian Crossing Recommended in the Conceptual Streetscape Design



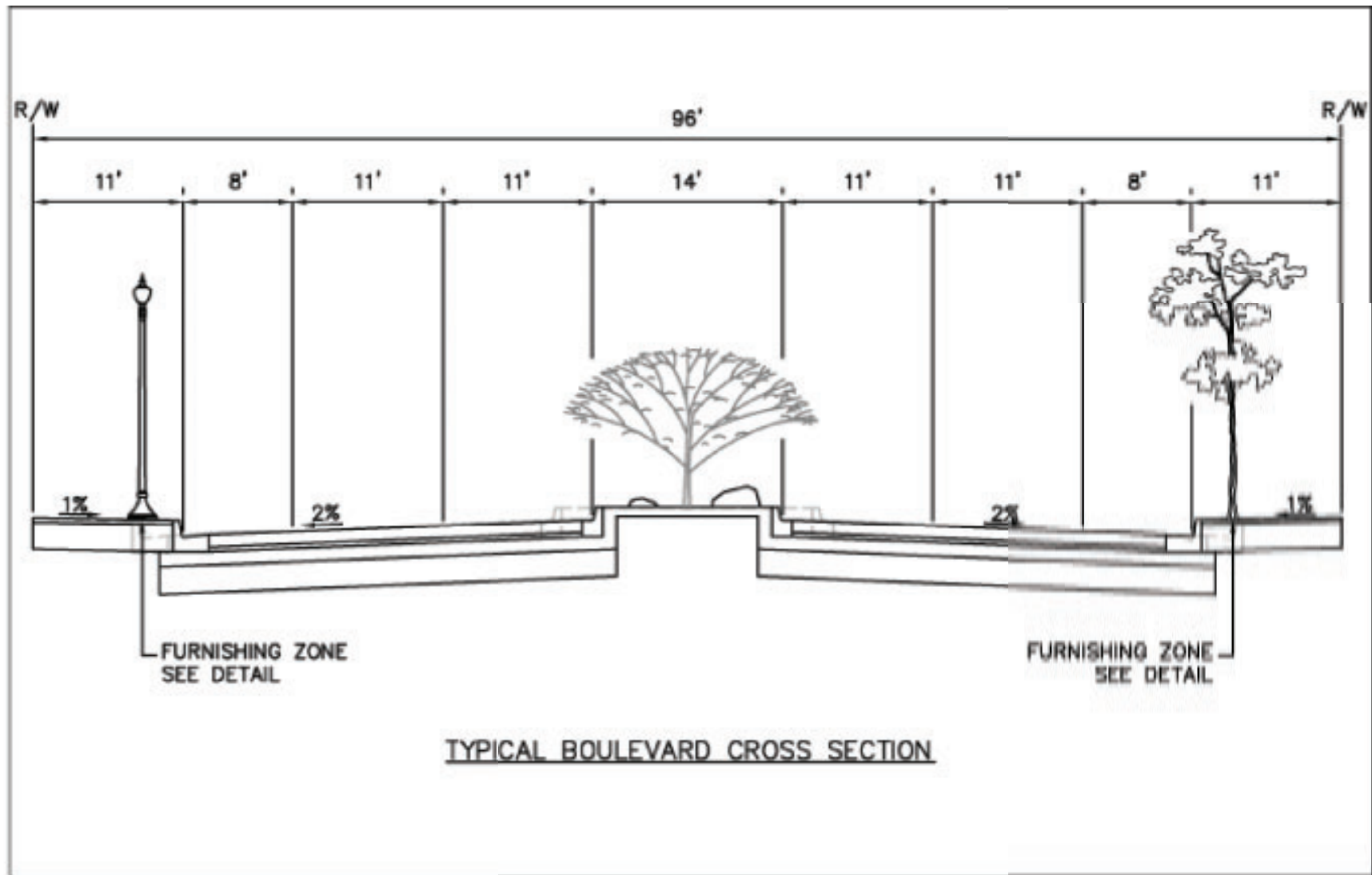


Figure D1



Figure D1: Typical Boulevard Cross Section

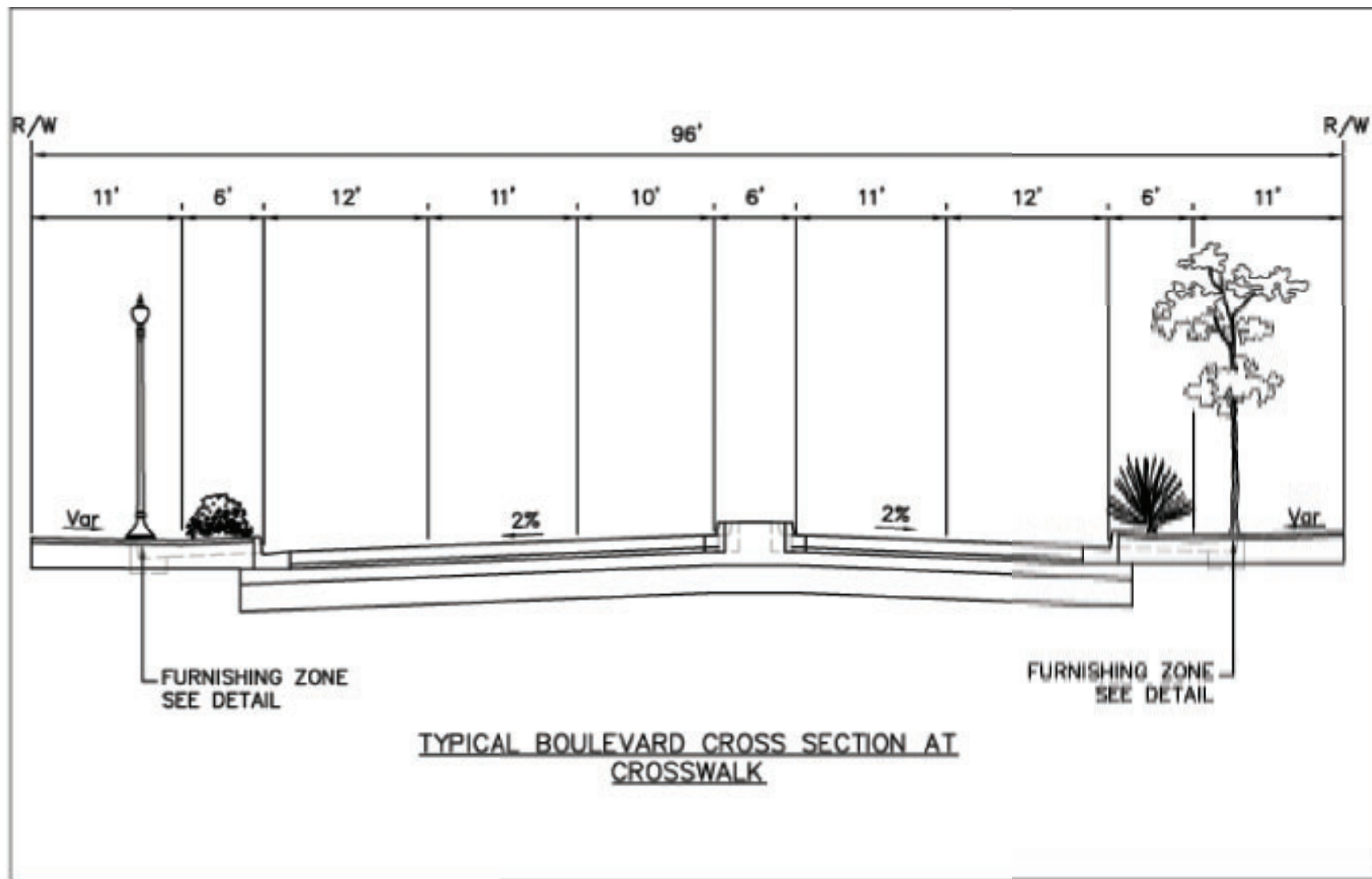


Figure D2



Figure D2: Typical Boulevard Cross Section at Crosswalk

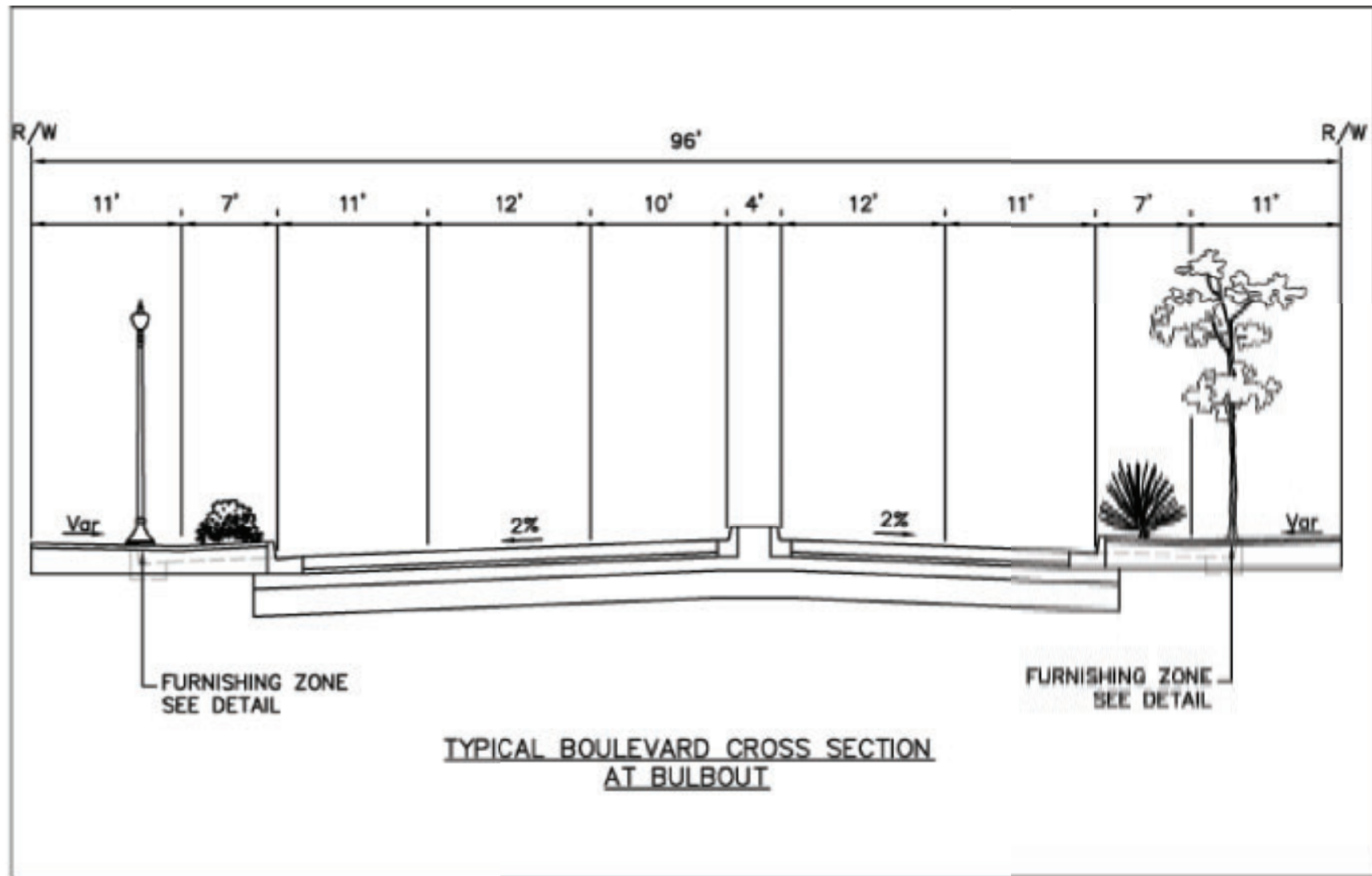


Figure D3



Figure D3: Typical Boulevard Cross Section at Bulbout

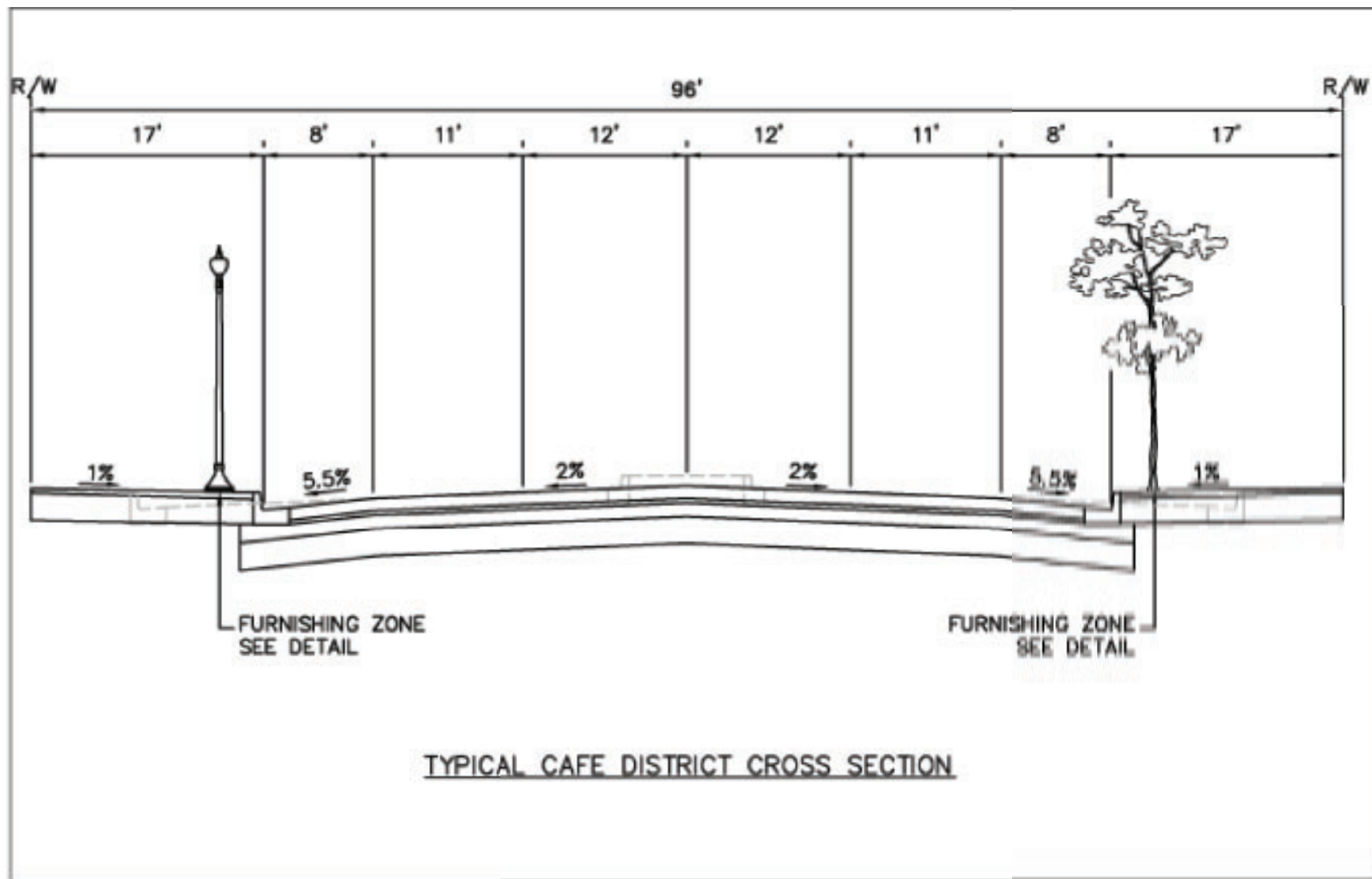


Figure D4



Figure D4: Typical Café District Cross Section

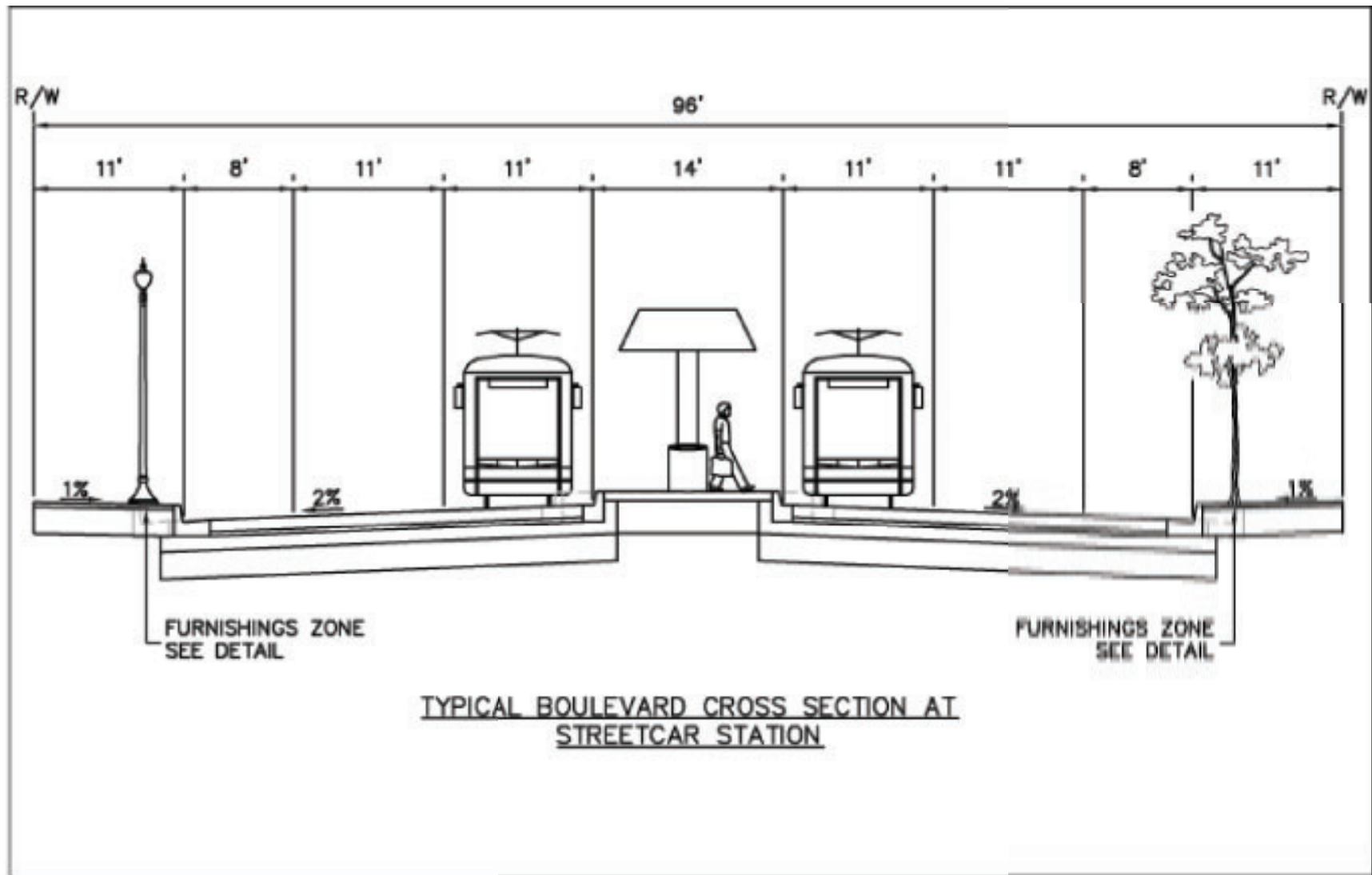


Figure D5



Figure D5: Typical Boulevard Cross Section at Streetcar Station

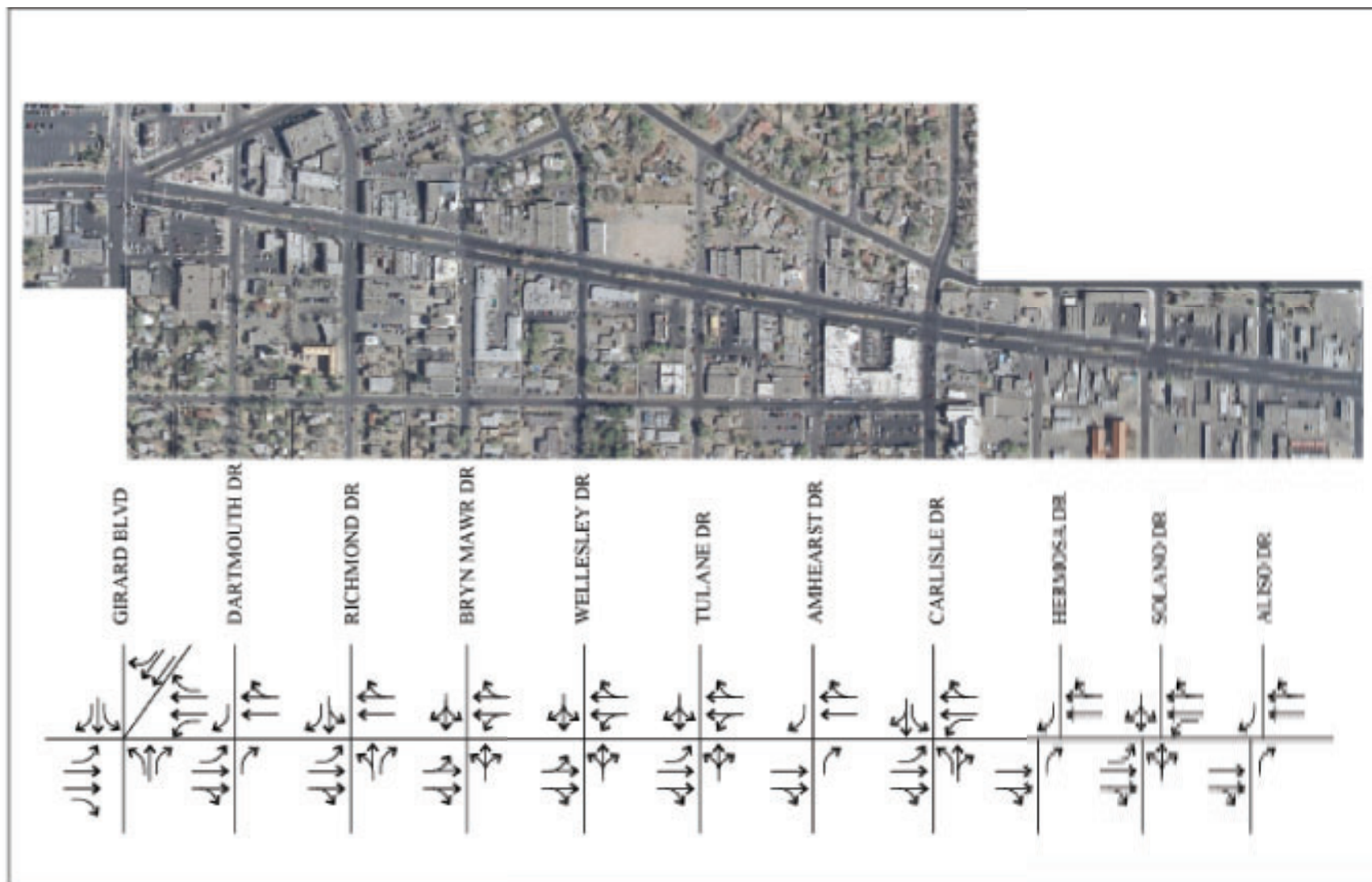


Figure D6

Intersection Turn Movements



Figure D6: Intersection Turn Movements

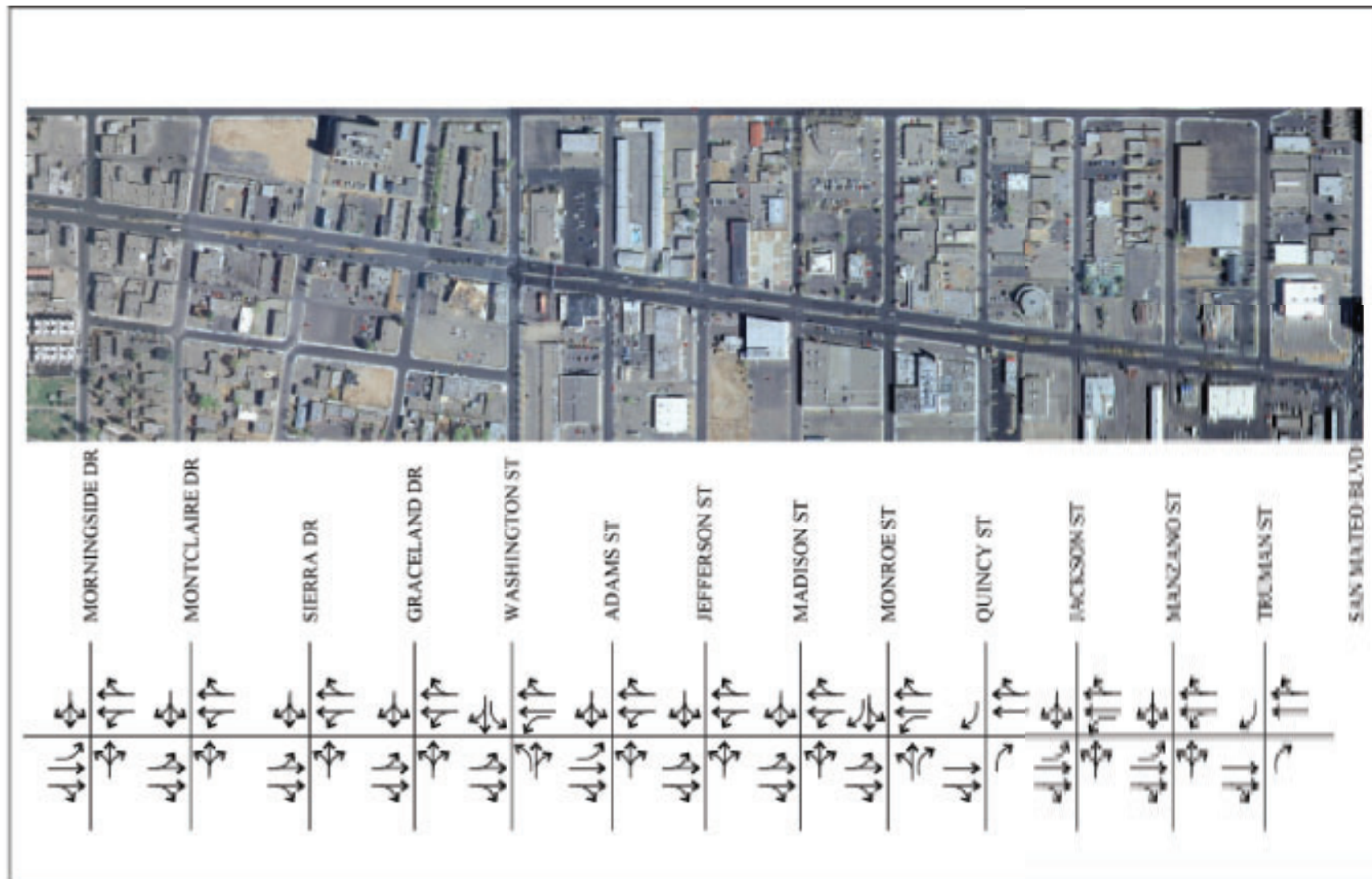


Figure D7

Intersection Turn Movements (cont.)



Figure D7: Intersection Turn Movements (Cont.)



Figure D8



Figure D8: Prototypical Signalized Intersection

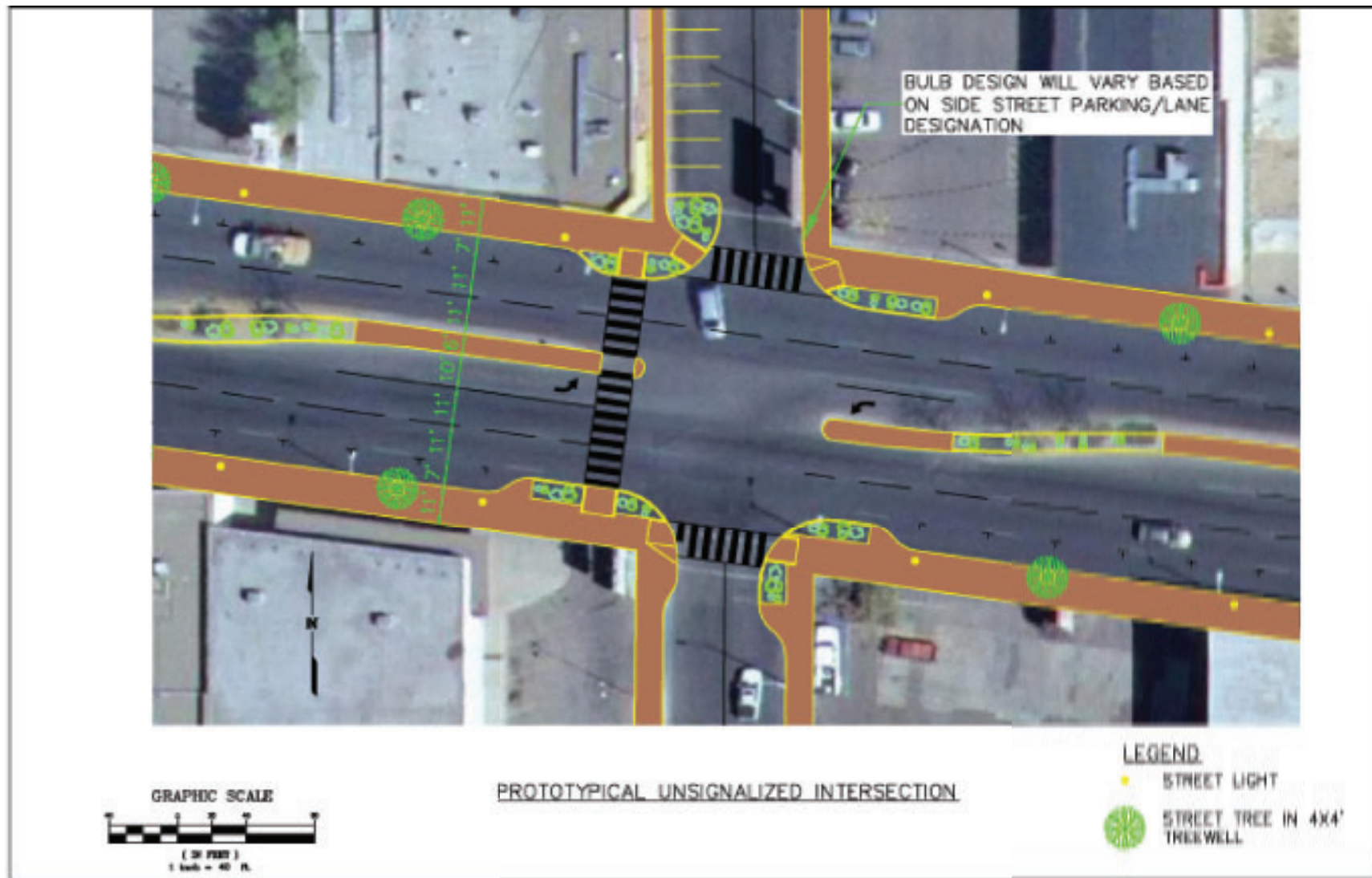


Figure D9



Figure D9: Prototypical Unsignalized Intersection

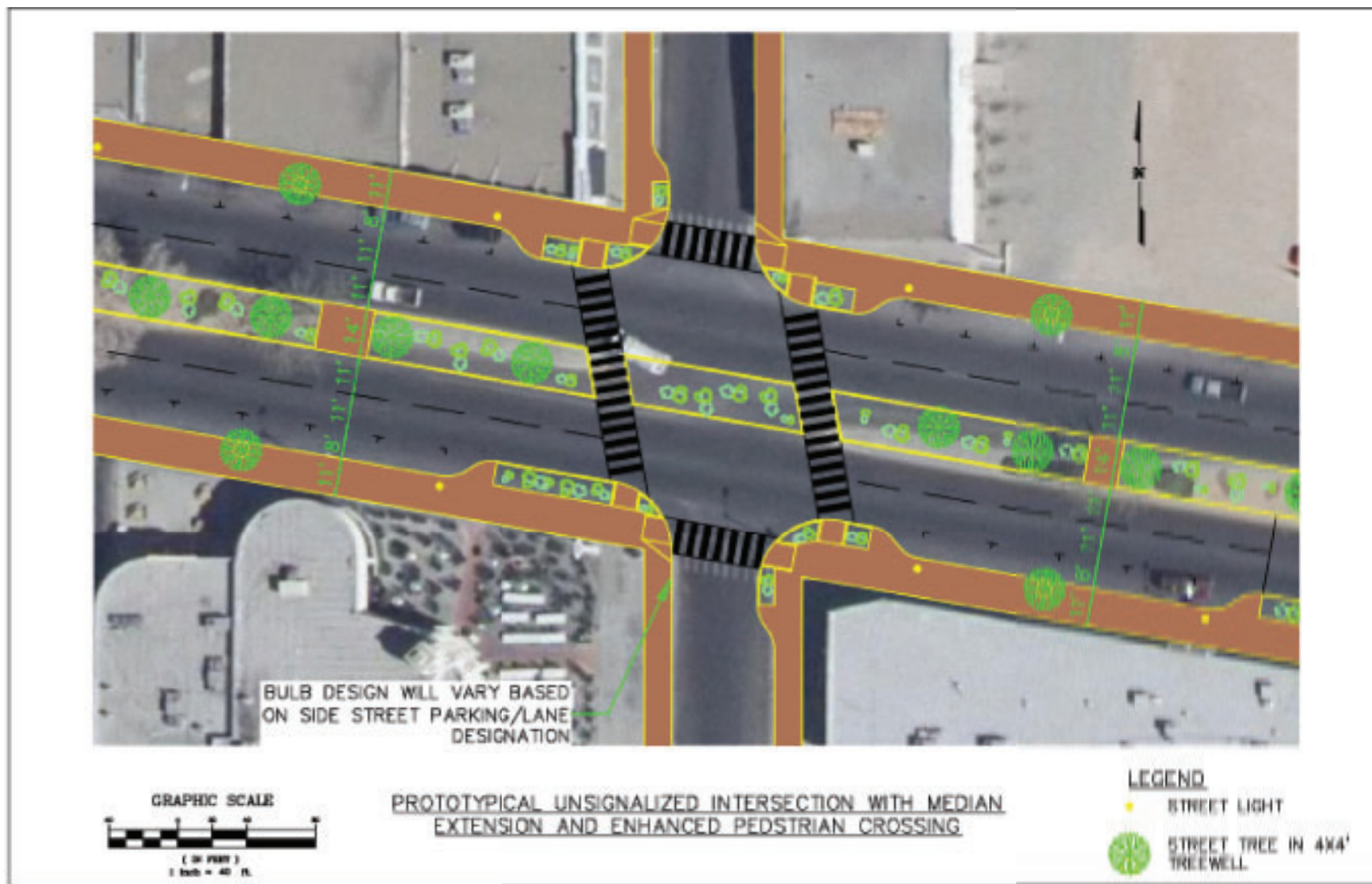


Figure D10



Figure D10: Prototypical Unsignalized Intersection With Median Extension and Enhanced Pedestrian Crossing

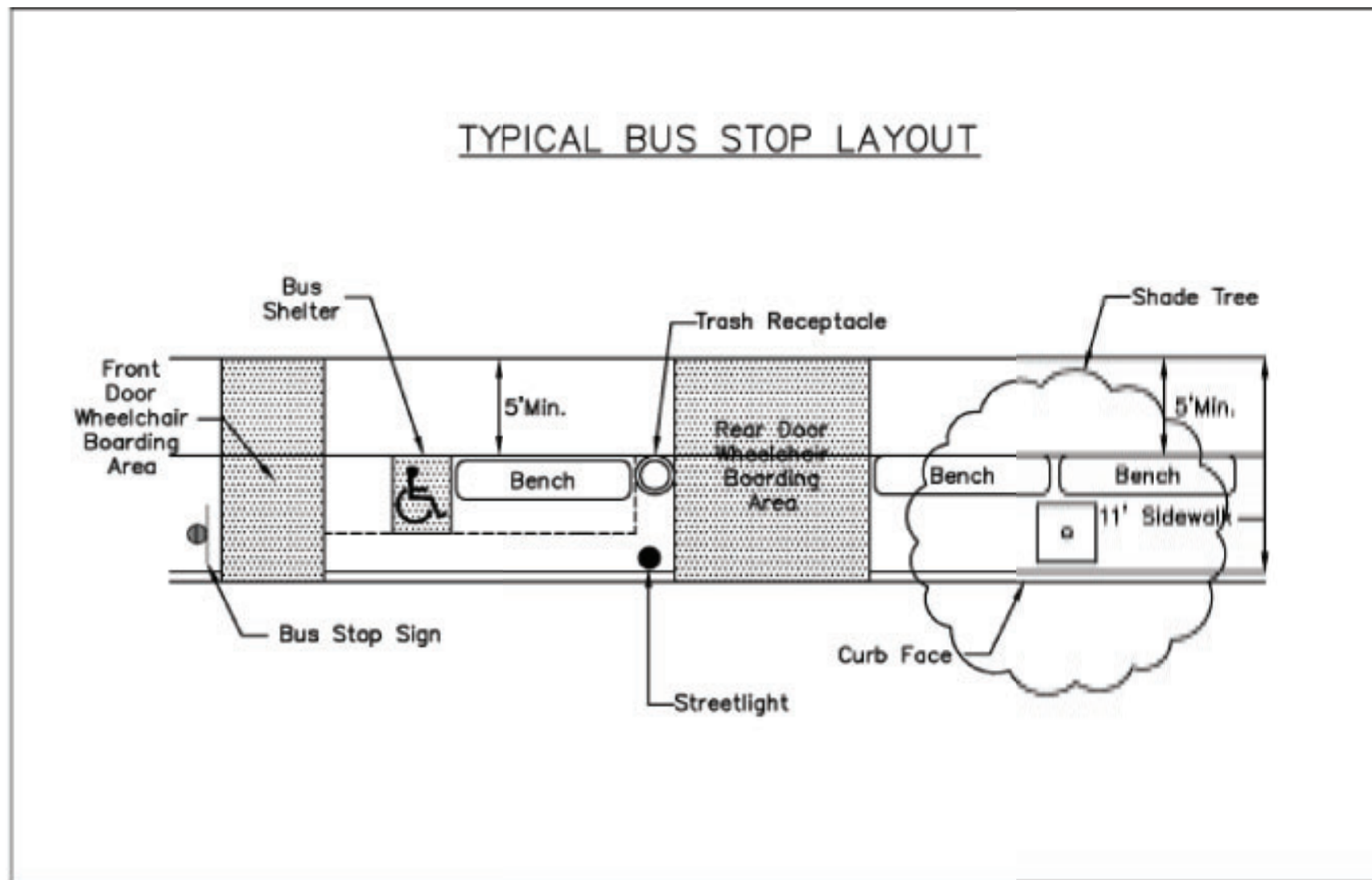


Figure D11



Figure D11: Typical Bus Stop Layout